

WEST WEBER PROPERTY  
DESIGN CODE

PLACEHOLDER FOR  
ILLUSTRATION

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*by*  
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*for*  
BLACKPINE  
WEBER COUNTY, UTAH



# INTRODUCTION

## Intent

The intent of this Design Code is to enable pedestrian-friendly design, with a fine-grained mix of civic spaces, thoroughfares, building types, and uses, generally arranged with distinct centers and edges. Good urban design takes pressure off of the architecture, allowing building designs to be simple and appropriate to the local context, in service of shaping a beautiful and pedestrian-friendly public realm.

The Community is designed as a collection of mixed-use, walkable neighborhoods with a range of uses which may include residential, lodging, office, retail, restaurant, and civic uses. The neighborhood will be built incrementally over time and is designed to be flexible to respond to the market.

### At the Scale of the Neighborhood:

- Various daily needs occur within a walk of most dwellings.
- The Transect Zones include a variety of distinct physical environments, providing meaningful choices to potential residents.
- An interconnected network of Thoroughfares is designed to disperse vehicular traffic and encourage bicycle and pedestrian activity.
- A range of Civic Spaces (parks, greens, squares, plazas, and pocket parks) is distributed throughout the Neighborhood.

- Most commercial and civic activity is embedded in the Neighborhood Center.
- A range of housing types to meet market demand.

### At the Scale of the Block and the Building:

- The design of Thoroughfares and buildings should reinforce safe environments, but not at the expense of accessibility.
- Buildings and landscaping should contribute to the physical definition of thoroughfares and civic spaces.
- Civic Buildings are distinctive and reflect their importance.
- The Neighborhood evolves harmoniously and orderly.
- Architecture and landscape design reflects climate, topography, history, and regional building practices.

Different people thrive in different places. There are those who could never live in an urban center; there are those who would wither in a rural hamlet. The rural-to-urban Transect is divided into six Transect Zones. These six zones vary by the level of intensity of their physical—and therefore social—character. Elements of the built environment are coordinated by these Transect Zones at all scales of planning, from the community to the individual lot and building.

This Neighborhood includes four of these Zones as well as a Special District.

**Rural (T2)** consists of low intensity development that, in this Community, is predominantly naturalistic Park space.

**The Neighborhood Edge Zone (T3)** consists of low density residential areas adjacent to more dense zones that may include a small amount of mixed-use. Planting is naturalistic and setbacks are relatively deep. Blocks may be large and the roads irregular to accommodate existing natural features.

**The Neighborhood General Zone (T4)** consists of a mixed-use but primarily medium residential density urban fabric. It may have a wide range of building types including houses, cottages, and townhouses. Setbacks and landscaping are variable. Streets with curbs and sidewalks define medium-sized blocks.

**The Neighborhood Center Zone (T5)** consists of higher density mixed-use buildings, apartments, and townhouses. It has a tight network of streets, with wide sidewalks, regular street tree planting and buildings close to the sidewalks.

**The Special District (SD)** consists of areas with buildings that by their Function, Disposition, or Configuration cannot, or should not, conform to one or more of the normative Transect Zones.



## INTRODUCTION: DEFINITIONS

**Accessory Building:** a building on the same Lot as a Principal Building usually located toward the rear.

**Accessory Dwelling Unit:** a dwelling unit sharing ownership and utility connections with a Primary Building.

**Allee:** a regularly spaced and aligned row of trees usually planted along a Thoroughfare or Path.

**Arcade:** *See Definitions: Illustrated.*

**Attic:** the part of a building contained within a pitched roof that, though it may be accessible, does not include habitable space.

**Awning:** a flexible roof-like cover that extends out from an exterior wall and shields a window, doorway, sidewalk, or other space below from the elements.

**Basement:** the interior part of a building (which may or may not include habitable space) below the ground of every story of main entry level, and which has a maximum of 5 feet of height above grade, measured from the average finished grade along the front Facade to the finished ceiling of the Basement.

**Bay Window:** a combination of window units which projects from a building.

**Block:** the aggregate of private Lots, Secondary Thoroughfares, and Parking Lots, circumscribed by Front Thoroughfares or Civic Spaces.

**Civic:** entities serving the following purposes are considered civic uses: arts, culture, education (including schools), government, recreation, religion, social services, transit, cemeteries, meeting halls, post offices, wedding chapels, and the like.

**Civic Building:** a building dedicated to Civic use(s).

**Civic Space:** an outdoor area dedicated for Civic use.

**Commercial:** the term collectively including Lodging, Office, and Retail Uses.

**Commercial Street (CS):** a local, slow-movement thoroughfare most suitable for general and center zones — typically where there are adjacent lodging, office, and/or retail uses. A Commercial Street is typically urban in character, with closed drainage, wide sidewalks, parallel parking, and trees in individual planting areas.

**Common Lawn:** *see Definitions: Illustrated.*

**Community:** the community proposed in this Design Code.

**Corner Lot:** a Lot with two intersecting Frontages.

**Cul-de-sac:** a vehicular Street that is closed at one end, often terminating in a vehicular turnaround.

**Curb:** the edge of the vehicular pavement that may be raised, mountable, or flush. It usually incorporates the drainage system.

**Density:** the number of dwelling units within a standard measure of area.

**Dwelling Unit:** a room or collection of rooms intended for residential occupancy in which sleeping quarters are provided, and kitchen and bathroom facilities are shared by occupants.

**Effective Turning Radius:** *see Definitions: Illustrated.*

**Elevation:** *see Definitions: Illustrated.*

**Encroach:** to break the plane of a vertical or horizontal regulatory limit with a structural element, so that it extends into a Setback, into the Public Frontage, or above a height limit.

**Encroachment:** any structural element that breaks the plane of a vertical or horizontal regulatory limit, extending into a Setback, into the Public Frontage, or above a height limit.

**Enfront:** to place an element along a Frontage, as in "porches Enfront the street."

**Facade:** *see Definitions: Illustrated.*

**Forecourt:** *See Definitions: Illustrated.*

**Front Thoroughfare:** a Thoroughfare that typically runs along the front of Private Lots and which creates Frontages.

**Frontage:** *see Definitions: Illustrated.*

**Frontage Line:** *see Definitions: Illustrated.*

**Gallery:** *see Definitions: Illustrated.*

**Green:** *see Civic Space Types.*

**Home Occupation:** non-retail commercial operation and/or remote work which takes place in a residence.

**Lodging:** premises available for short-term human habitation, including daily and weekly rental.

**Lot:** a parcel of land accommodating a building or buildings of unified design. The size of a Lot is controlled by its width in order to determine the grain (i.e., fine grain or coarse grain) of the urban fabric.

**Lot Coverage:** the percentage of lot area covered by buildings including Principal Buildings, Accessory Buildings, Porches, Stoops, and steps..

**Lot Line:** *see Definitions: Illustrated.*

**Lot Width:** the length of the Primary Frontage Line of a Lot.

**Main Entrance:** the main point of access for pedestrians into a building.

**Marquee:** an architectural projection serving as a roof, the sides of which are vertical and are intended for the display of signs, and which is supported entirely from an exterior wall of a building.

**Master Plan:** the proposed design for the Neighborhood as described through this Design Code and accompanying drawings.

**Office:** premises available for the transaction of general business including medical and dental offices, clinics and outpatient facilities, but excluding retail sales and manufacturing.

**Open Space:** land intended to remain undeveloped; it may be for Civic Space.

**Park:** *see Civic Space Types.*

**Parking Area:** portions of Private Lots intended for parking, including Parking Lots, garages, and Parking Structures.

**Parking Lane:** a portion of a Thoroughfare dedicated to vehicular parking.

**Parking Structure:** a building containing one or more Stories of parking above grade.

**Passage (PS):** a pedestrian connector, open or roofed, that passes between buildings to provide shortcuts through long Blocks and often to connect rear Parking Areas to a Frontage.

**Path (PT):** a pedestrian way of traversing a Park or rural area, with landscape matching the contiguous Open Space, ideally connecting directly with the urban Sidewalk network.

**Pavement Radius:** *see Definitions: Illustrated.*

**Planting Strip:** in Public Frontage, the element which accommodates street trees, whether continuous or individual.

**Plaza:** *see Civic Space Types.*

**Principal Building:** the main building on a Lot, usually located toward the Frontage.

**Primary Entrance:** the main point of access for pedestrians into a building.

**Primary Frontage:** *See Definitions: Illustrated. See Frontage.*

**Private Frontage:** *See Definitions: Illustrated. See Frontage.*

**Public Frontage:** *See Definitions: Illustrated. See Frontage.*

**Rear Thoroughfare:** a Thoroughfare that typically runs along the rear of Private Lots and does not create Frontages.

**Regulating Plan:** a map or set of maps that shows the Transect Zones, Special Districts if any, and Special Requirements if any, of areas subject to, or potentially subject to, regulation by this Design Code.

**Residential:** premises available for long-term human habitation by means of ownership or rental for more than 30 days.

**Retail:** premises available for the commercial sale of merchandise, services, and prepared foods (including restaurants).

**Right-of-Way (ROW):** *See Definitions: Illustrated.*

**Road (RD):** a local, rural and suburban Thoroughfare of low-to-moderate vehicular speed and capacity.

**Secondary Frontage:** *See Definitions: Illustrated. See Frontage.*

**Setback:** the area of a Lot measured from the Lot line to a building Facade or Elevation that is maintained clear of permanent structures, with the exception of Encroachments listed on the Urban Standards.

**Shared-Use Path:** a path intended for use by pedestrians, cyclists, and other micro-mobility users.

**Sidewalk:** the paved section of the Public Frontage dedicated to pedestrian activity.

**Site:** the entire area to which this Design Code applies.

**Special District (SD):** an area that inherently cannot or should not conform to one or more of the normative Transect Zones due to its auto-oriented design and uses, but is nonetheless incorporated into the Neighborhood.

**Square:** *see Civic Space Types.*

**Stoop:** *see Definitions: Illustrated.*

**Storefront:** *see Definitions: Illustrated.*

**Story:** a habitable level within a building, excluding an Attic or Basement.

**Street (ST):** a local urban Thoroughfare of low-to-moderate speed and capacity.

**Thoroughfare:** *see Definitions: Illustrated.*

**Thoroughfare Component:** one of the elements which can be assembled with others into a Thoroughfare. These include Vehicular Travel Lanes, Parking Lanes, Medians, Sidewalks, pedestrian Paths, Shared-Use Paths, and Planting Strips.

## INTRODUCTION: DEFINITIONS

**Tower:** a portion of a structure which exceeds the parapet or ridge height of the attached building.

**Transect:** a cross-section of the environment showing a range of different habitats. The rural-urban Transect of the human environment used in this Design Code is divided into six Transect Zones. These zones describe the physical form and character of a place, according to the Density and intensity of its land use and Urbanism.

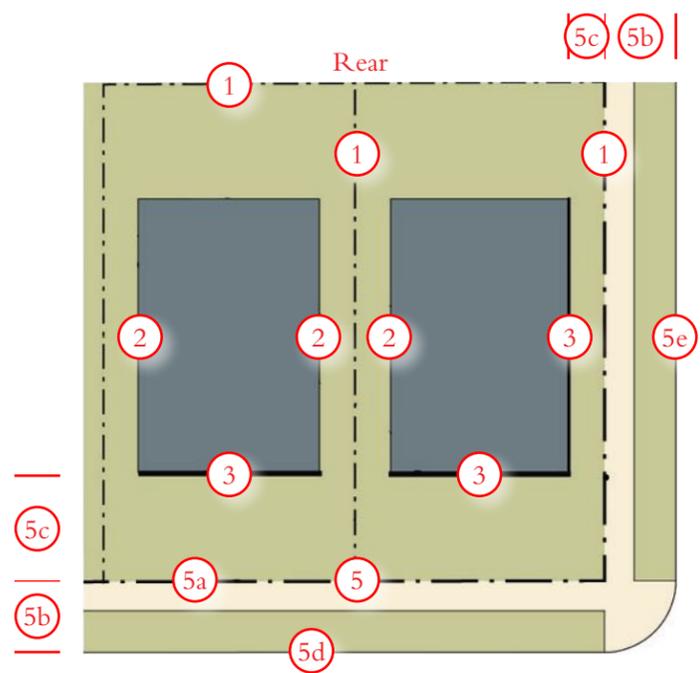
**Transect Zone:** one of several areas on a Zoning Map regulated by this Design Code. Transect Zones are administratively similar to the land use zones in conventional codes, except that in addition to the usual building use, density, height, and Setback requirements, other elements of the intended habitat are integrated, including those of the private Lot and building and Public Frontage.

**Turning Radius:** the curved edge of a Thoroughfare at an intersection, measured at the inside edge of the vehicular tracking. The smaller the Turning Radius, the smaller the pedestrian crossing distance and the more slowly the vehicle is forced to make the turn.

**Use:** the function or functions accommodated by a building and its Lot, categorized as Restricted, Limited, or Open, according to the intensity of the use.

**Vehicular Travel Lane:** a portion of a Thoroughfare dedicated to vehicular movement.

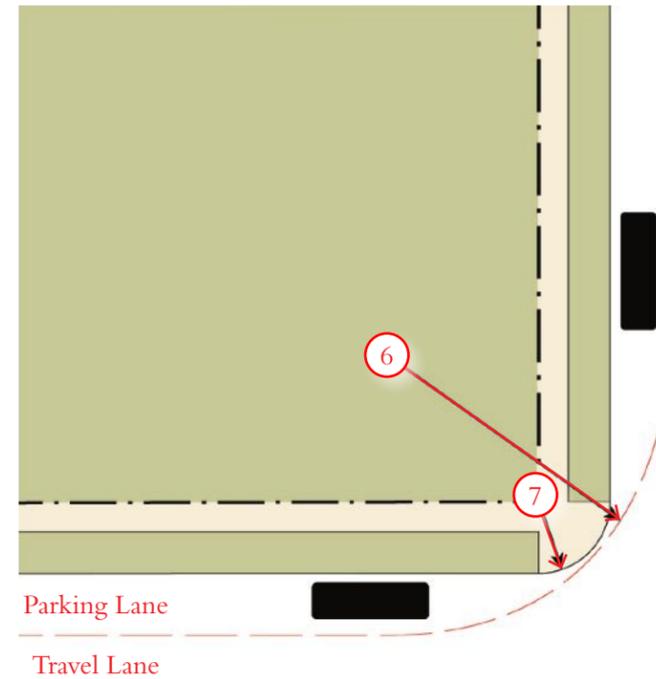
# INTRODUCTION: DEFINITIONS, ILLUSTRATED



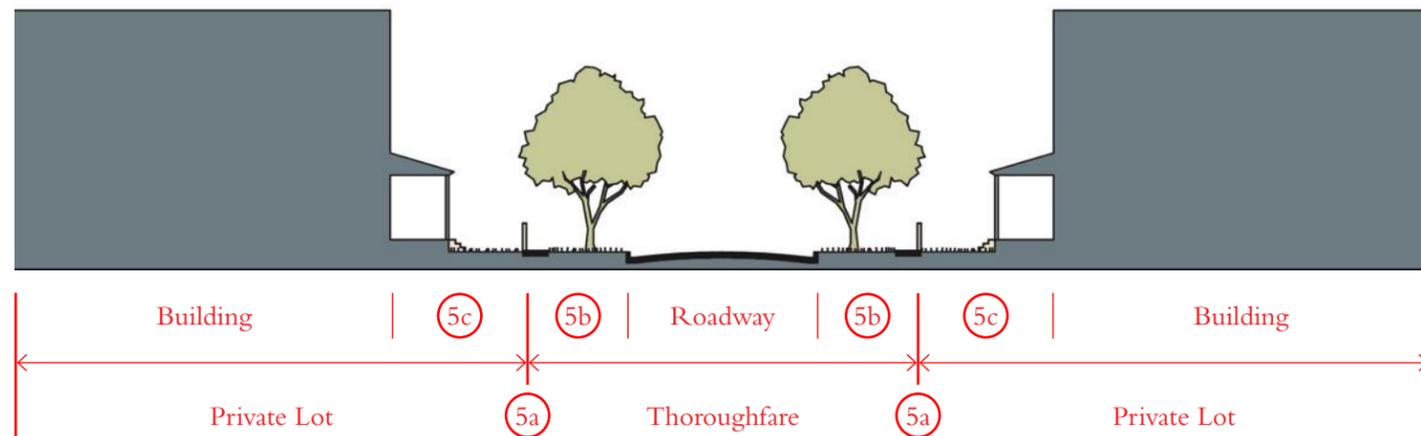
- 1 **Lot Line:** the boundary that legally and geometrically demarcates a lot.
- 2 **Elevation:** an exterior wall of a building not facing a frontage line.
- 3 **Facade:** the exterior wall(s) of a building that is (are) set along a frontage line. Facades face frontage lines and define the public realm, and are therefore more regulated than the elevations facing other lot lines.
- 4 **Thoroughfare:** the composite public area dedicated to circulation, including vehicular lanes (if any), pedestrian paths and walks, public frontage.

5 **Frontage:** the area between a building facade and a thoroughfare or civic space, including built and planted components. Frontage is divided into public frontage and private frontage.

- a **Frontage Line:** the line between the public and private frontage. The frontage line is coincident with the property line(s), at frontages.
- b **Public Frontage:** the area between the edge of vehicular lanes, if present, and the frontage line, which is within the right-of-way. Typically, the public frontage consists of the planter and the sidewalk.
- c **Private Frontage:** the area between the frontage line and the principal building facade, which is within the private lot.
- d **Primary Frontage:** on lots with multiple frontages, the frontage which will bear the address and primary entrance to the building.
- e **Secondary Frontage:** on lots with multiple frontages, the frontage(s) that are not the primary frontage.



- 6 **Effective Turning Radius:** the measurement of the inside turning radius taking parked cars into account.
- 7 **Pavement Radius:** the curved edge of a vehicular lane at an intersection, measured at the inside edge of the travel lane if there is no on-street parking, or the parking lane if there is. The smaller the pavement radius, the more slowly the vehicle is forced to make the turn.



## INTRODUCTION: DEFINITIONS, ILLUSTRATED

**Common Lawn:** a facade set back substantially from the frontage line. The front yard should remain unfenced and be visually continuous with adjacent front yards. The intention is to emulate the character of buildings sitting in a common rural landscape. A front porch is encouraged but not required, as social interaction from the sidewalk is unlikely at such a distance.



**Porch:** a facade is set back from the frontage line with a front porch. The porch should be within a conversational distance of the sidewalk. Hedges, fences, or garden walls at the frontage line are encouraged to define the front yard. A great variety of porches is possible, but to be useful, none should be less than 6 feet deep.



**Terrace & Light Court:** a facade is set back from the frontage line with an elevated garden or terrace, or a sunken light court. This type can effectively buffer residential uses from the sidewalk, by creating an elevated front garden. The terrace created may also be suitable for restaurants and cafes, as the eye of the sitter is level with that of the standing passerby. The light court can give light and access to a basement.



**Forecourt:** a facade is aligned close to the frontage line with a portion of it set back. The forecourt created is suitable for a garden or plaza, providing a transition from the public sidewalk to the building entry. This type should be used sparingly. Trees within the forecourts should be placed to have their canopies overhanging the sidewalks.



**Stoop:** a facade is aligned close to the frontage line with the ground story elevated from the sidewalk, securing privacy for the windows. This type is suitable for buildings with ground-floor residential uses, such as townhouses and apartment buildings. This type may be interspersed with the storefront.



**Storefront:** a facade is aligned close to the frontage line with the entrance at sidewalk grade. This type is conventional for retail frontage. It is commonly equipped with a cantilevered shed roof or an Awning. The absence of a raised ground story generally precludes residential uses on the ground floor, although residential uses may be present where retail is anticipated in the future.



**Gallery:** a roof extends over the sidewalk above while the building facade remains at the frontage line. This type is most common for office, retail, and manufacturing uses. To be useful, the gallery should be no less than 12 feet deep and should overlap the sidewalk to within 2 feet of a vehicular lane so that a pedestrian cannot bypass it.



**Arcade or Colonnade:** An arcade (arches) or colonnade (columns and beams) is a structure including habitable space extending over a sidewalk supported by arches and open to the adjacent street or civic space. The ground story remains at the frontage line while the facade of the upper stories overlaps the sidewalk. This type is most common for office, retail, and manufacturing uses.



# INTRODUCTION

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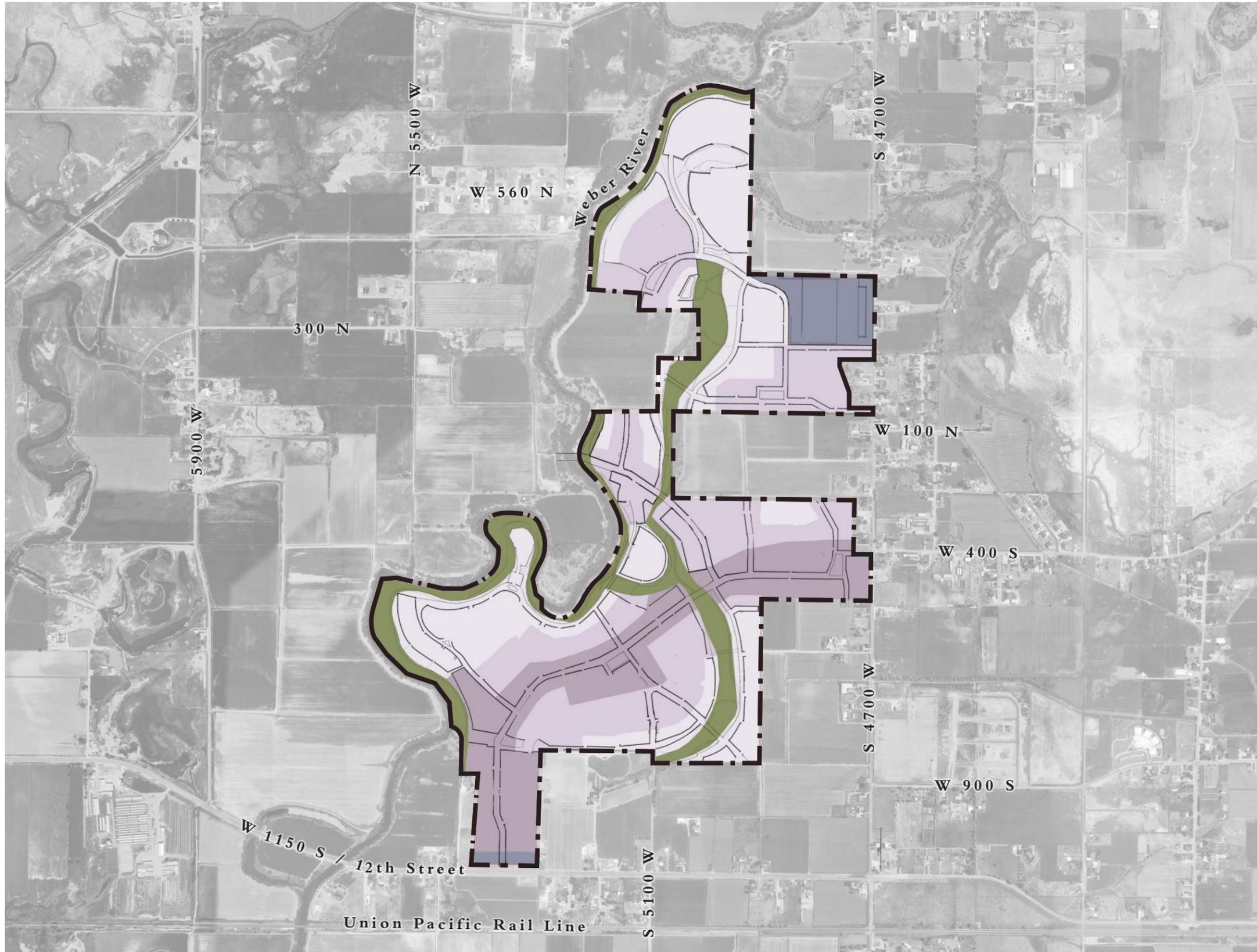
## Administration

1. **Conformity.** It is the Intent not the "letter" of this Design Code to which Thoroughfares, Civic Spaces, Private Lots, utilities, and other improvements shall conform.
2. **Typical and Actual Condition.** This Design Code are typologically-based and this represents the typical condition. It is expected that actual conditions will vary from the typical conditions shown. All components of Thoroughfares, Civic Spaces, and Private Lots are expected to be refined to be appropriate for specific locations in a reasonable manner that achieves the intent of this Design Code.
3. **Town Architect.** The Town Architect serves as the keeper of the urban and architectural vision of the Neighborhood. The Town Architect provides direction and inspiration to developers, lot purchasers, architects, designers, landscape architects, and builders.
  - a. **Specific Duties.** All plans for improvements within the Neighborhood shall be reviewed by the Town Architect to confirm compliance with the intent of this Design Code. The Town Architect may modify or amend this Design Code as permitted by the Master Developer Agreement for the Project.
  - b. **Design Review.** Given their familiarity with the principles of traditional neighborhood and architectural design, the Town Architect shall review all building designs prior to the issuing of a building permit by the County in place of design review by the Weber County Planning Commission.

## Neighborhood Structure

1. **Applicability.** The requirements of this section apply to the configuration of the Master Plan including future refinements therein.
2. **Transect Zones.** Transect Zones shall be assigned to all portions of the Site as mapped on the Regulating Plan (page 8).
  - a. **Adjustments to Area of Transect Zones.** The location of boundaries between Transect Zones may be adjusted to accommodate future adjustments to the Master Plan.
3. **Block Size.** On average, Blocks shall be 660 feet long in each direction.
4. **Thoroughfare Network.** Thoroughfares shall be laid out in an interconnected network to disperse vehicular traffic and promote walking and cycling. Given the intention of creating an interconnected network of Thoroughfares, connections to adjacent properties are proposed as noted on the Regulating Plan (page 8).
  - a. **Section Line & Quarter Section Line Streets.** Streets shall not be required along section lines and quarter section lines.
5. **Allocation of Civic Spaces.** A minimum of 10% of the total area of the Site shall be allocated to Civic Spaces. For the purposes of calculating this area, the Public Frontage of adjacent Thoroughfares may be counted towards the area of a Civic Space.
6. **Private Lots.** Buildings shall be located on Private Lots as consistent with the requirements of the Private Realm section of this Design Code (pages 26-30). Multiple Principal Buildings with associated Accessory Buildings may also be located on one legally defined parcel (e.g. an apartment complex of multiple buildings) as long as they are placed as if the Principal Buildings are on separate Lots.
7. **Access to Lots.** Public access to Private Lots may be provided via publicly accessible sidewalks, paths, passages, or similar means. Emergency vehicle access shall be provided through Front or Rear Thoroughfares. Lots shall also include a means of automobile access, with exceptions permitted by the Town Architect.
8. **Addressing.** Lots shall be addressed to an adjacent Front Thoroughfare or Civic Space, including exclusively pedestrian Front Thoroughfares.
9. **Utilities.** Utilities shall typically be located in Thoroughfares (as described in the Thoroughfare Standards on page 22) or in Civic Spaces. Location of utilities in easements on Private Lots shall be the exception and the location of last resort.
10. **Buffers.** No Buffers shall be required between different Uses, Lots, or any other components within the Master Plan.
11. **Perimeter Landscaping & Fencing.** Landscaping or fencing along the perimeter of the Neighborhood shall not be required except where adjacent to agricultural Uses and to screen Parking Areas.
12. **Fire Apparatus Access.** Fire apparatus access shall be provided to all buildings as consistent with the International Fire Code (IFC) as adopted by the County.
13. **Density & Intensity of Uses.** All Transect Zones are mixed-use and the proposed Uses in each phase may be adjusted as consistent with this Design Code. The Residential density of the Neighborhood shall be a maximum of 6 units per gross acre.

URBAN DESIGN: REGULATING PLAN



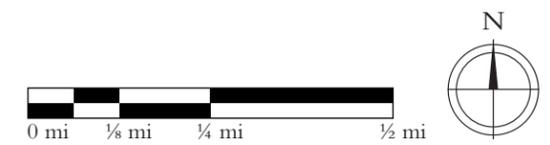
**Key**

-  Subject Property
-  T2: Rural Zone
-  T3: Suburban Zone
-  T4: General Urban Zone
-  T5: Urban Center Zone
-  SD: Special District

**Notes**

1. The Blocks shown on this plan will be divided by additional Thoroughfares and Civic Spaces into smaller Blocks as consistent with this Design Code.

The design is subject to change. Not for construction.



**PUBLIC REALM: THOROUGHFARE TYPES PLAN**

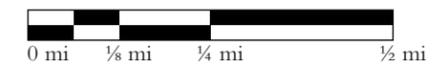


**Key**

- RD-44-20-B
- DR-46-27
- YS-53-27
- ST-46-20
- ST-53-27
- ST-60-34
- CS-52-28
- AV-80-36
- AV-80-36-C

**Notes**

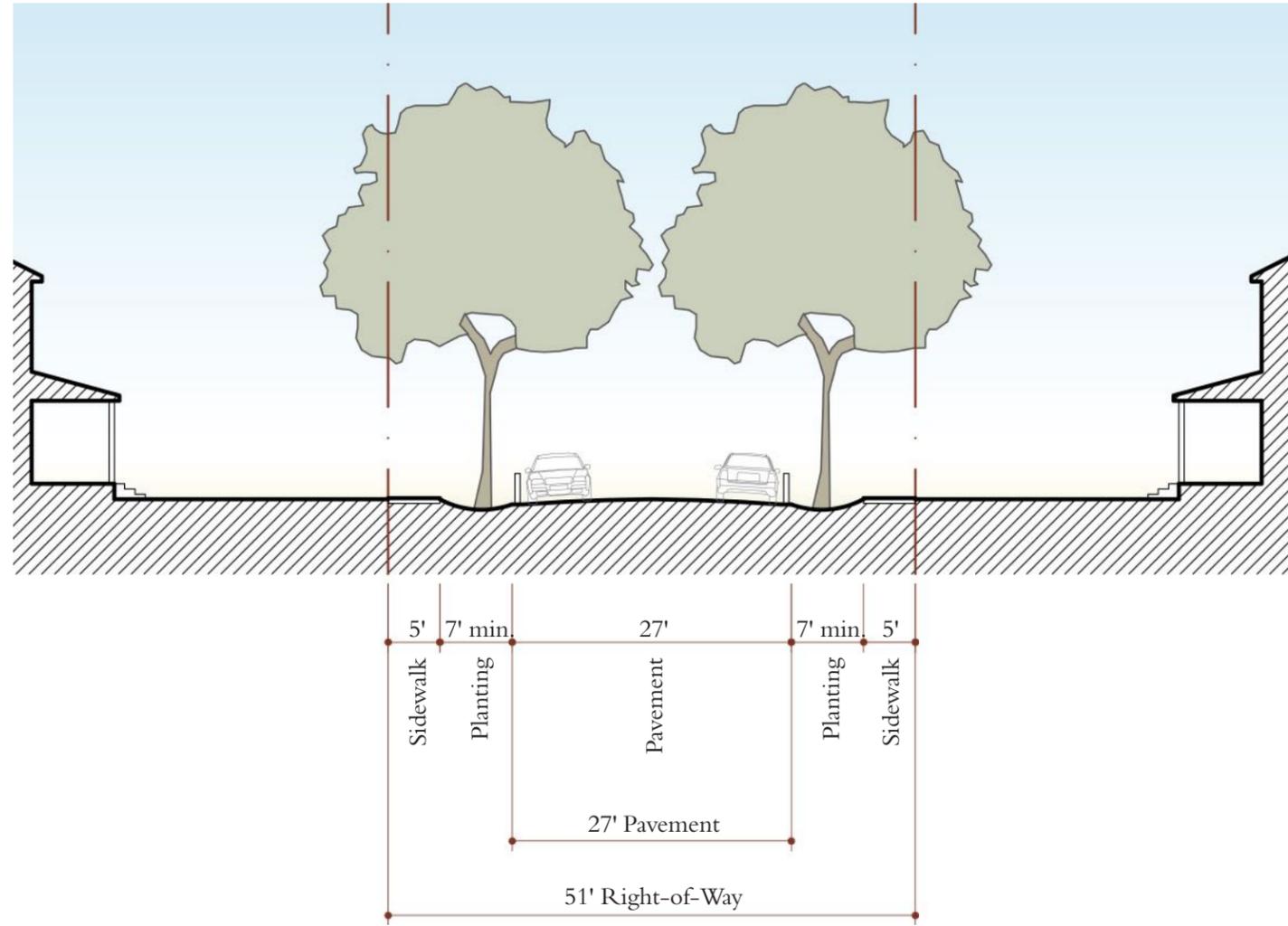
1. Thoroughfares will be added as consistent with this Design Code.



*The design is subject to change. Not for construction.*

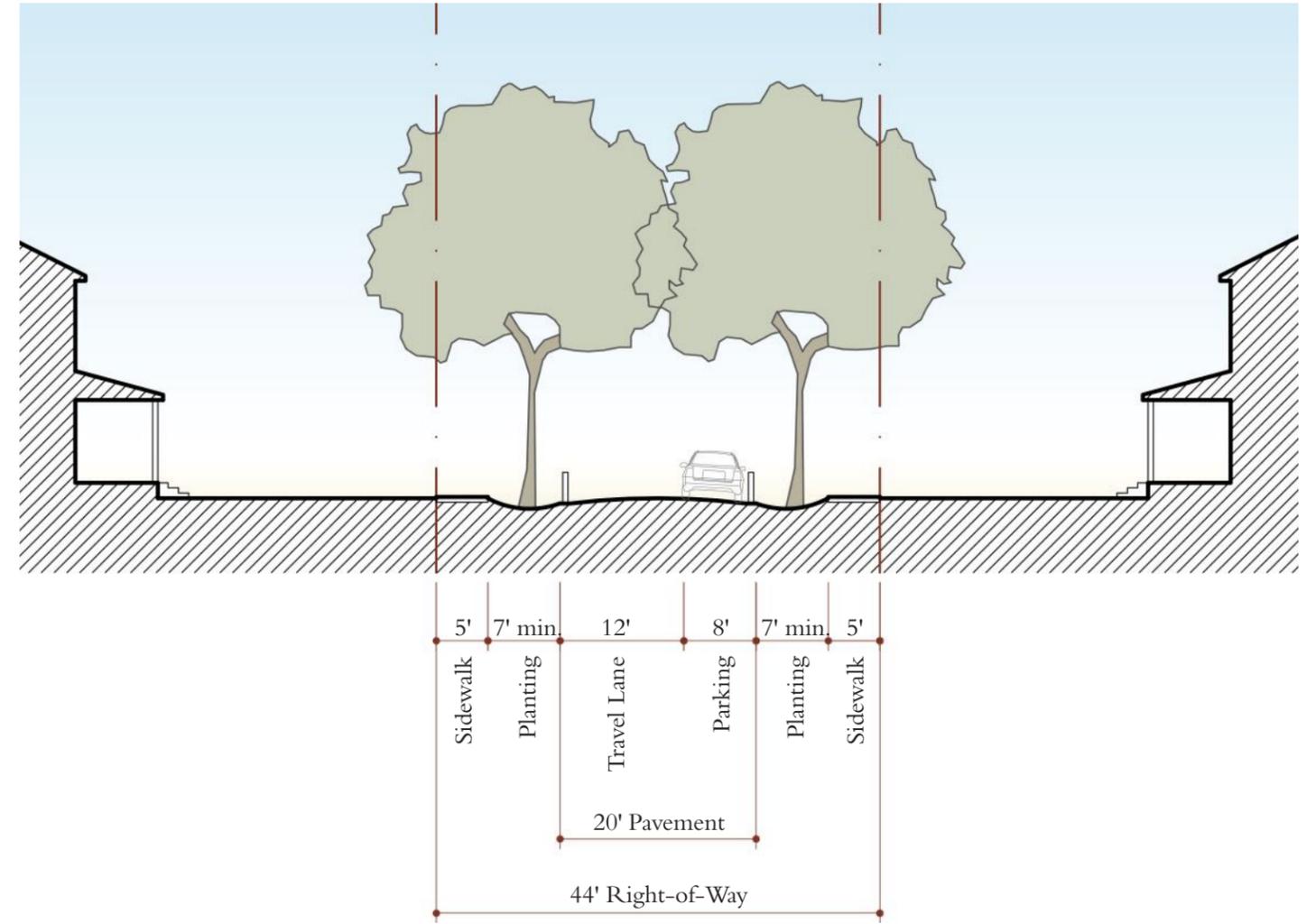


**PUBLIC REALM: THOROUGHFARE TYPES**



**YR-51-27**

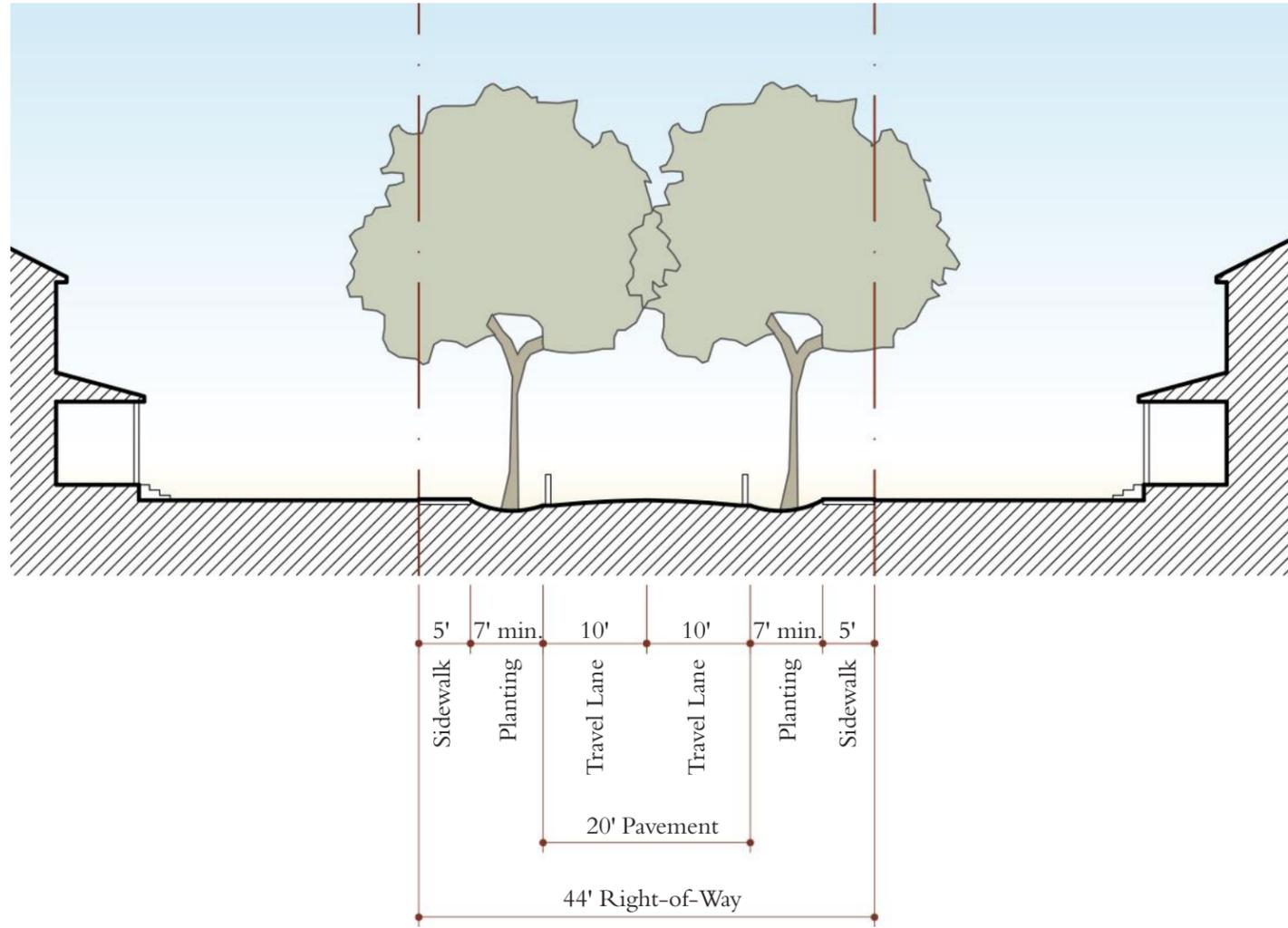
|  |             |                           |                                    |
|--|-------------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Yield Road  | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public      | <b>Curb Type:</b>         | Ribbon or Rolled                   |
| <b>Vehicular Movement:</b>                       | 2-way Yield | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph      | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | Varies      |                           |                                    |



**RD-44-20-A**

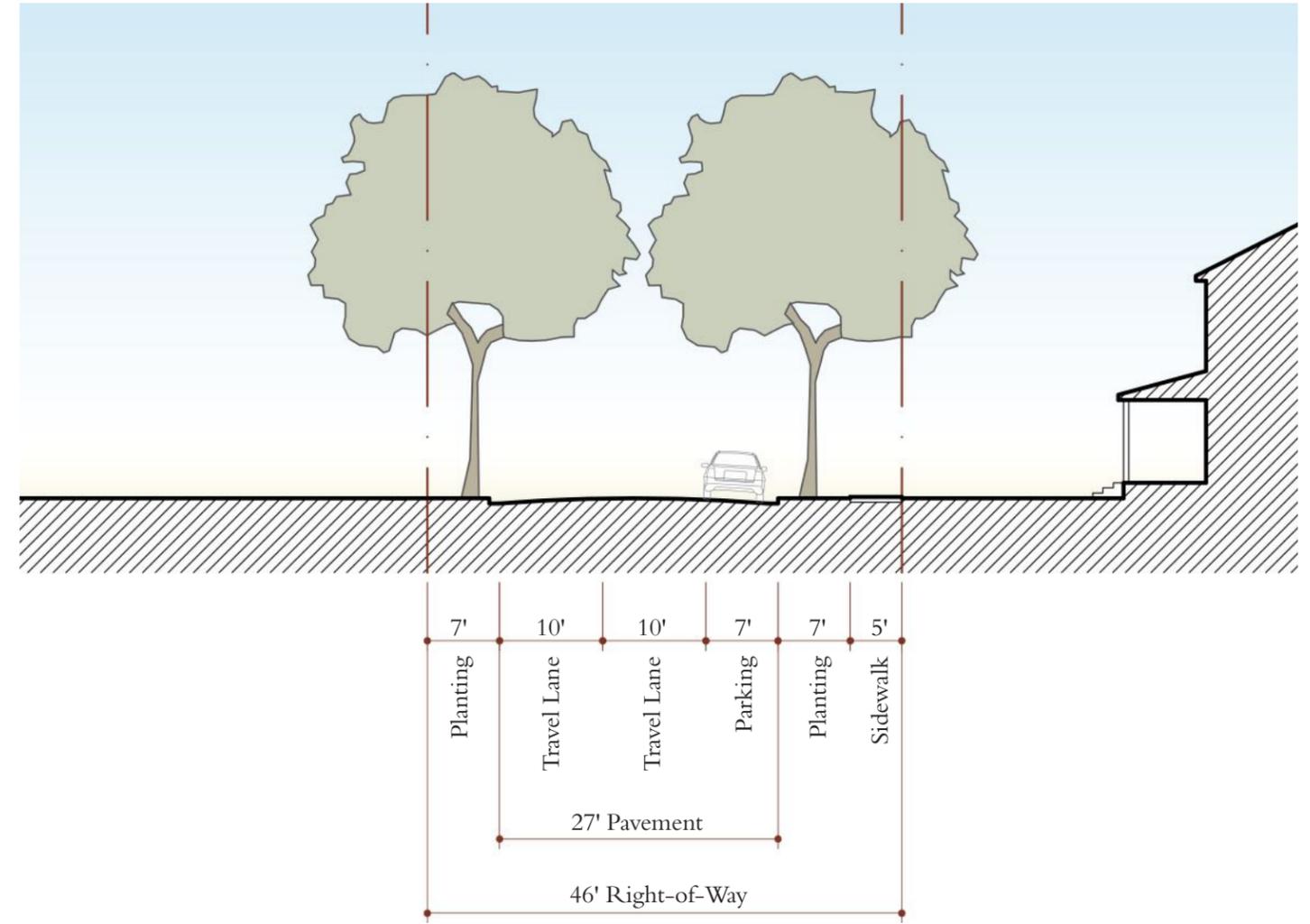
|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Road   | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | Ribbon or Rolled                   |
| <b>Vehicular Movement:</b>                       | 1-way  | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 12 ft. |                           |                                    |

**PUBLIC REALM: THOROUGHFARE TYPES**



**RD-44-20-B**

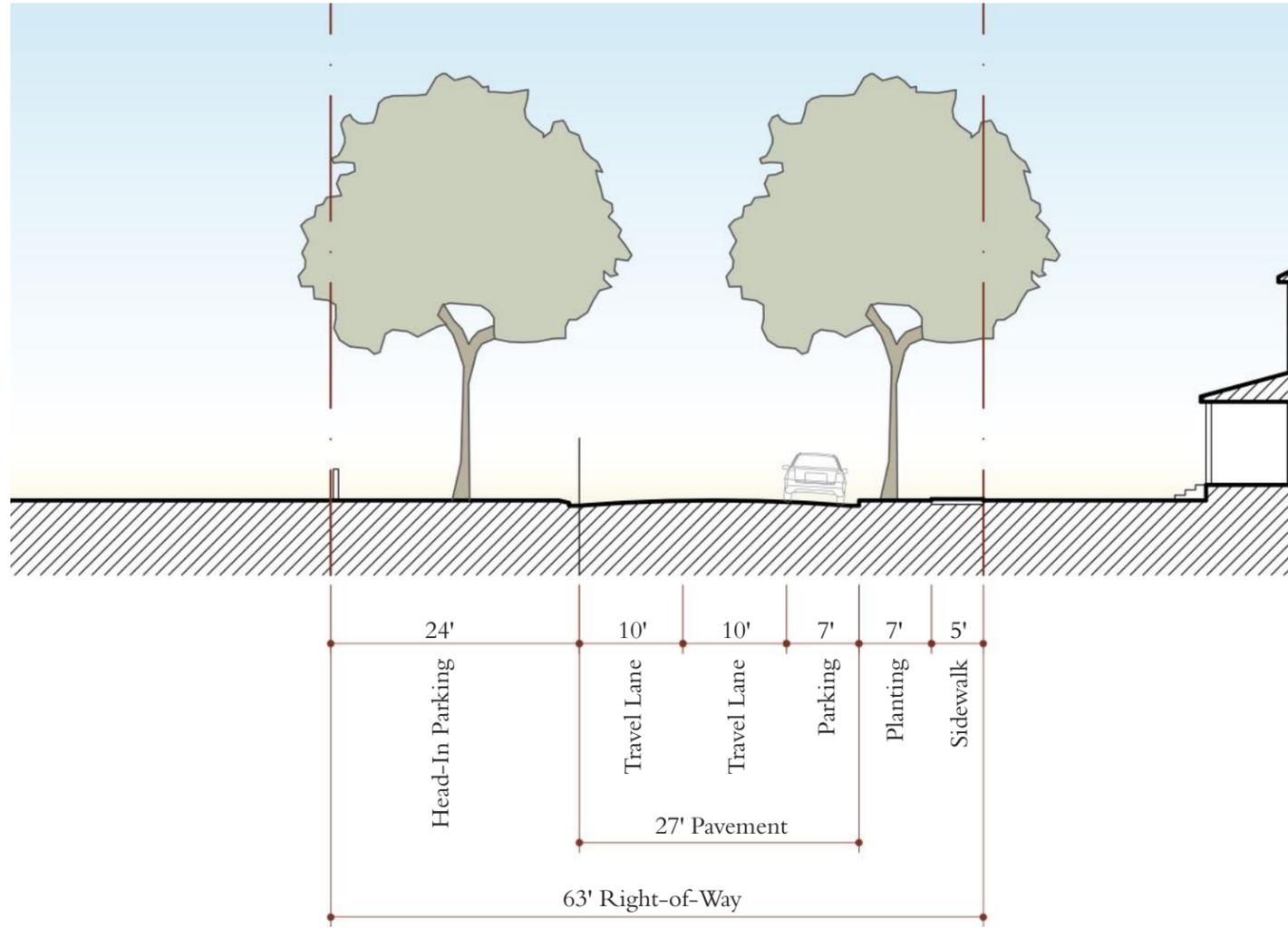
|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Road   | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | Ribbon or Rolled                   |
| <b>Vehicular Movement:</b>                       | 2-way  | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 20 ft. |                           |                                    |



**DR-46-27**

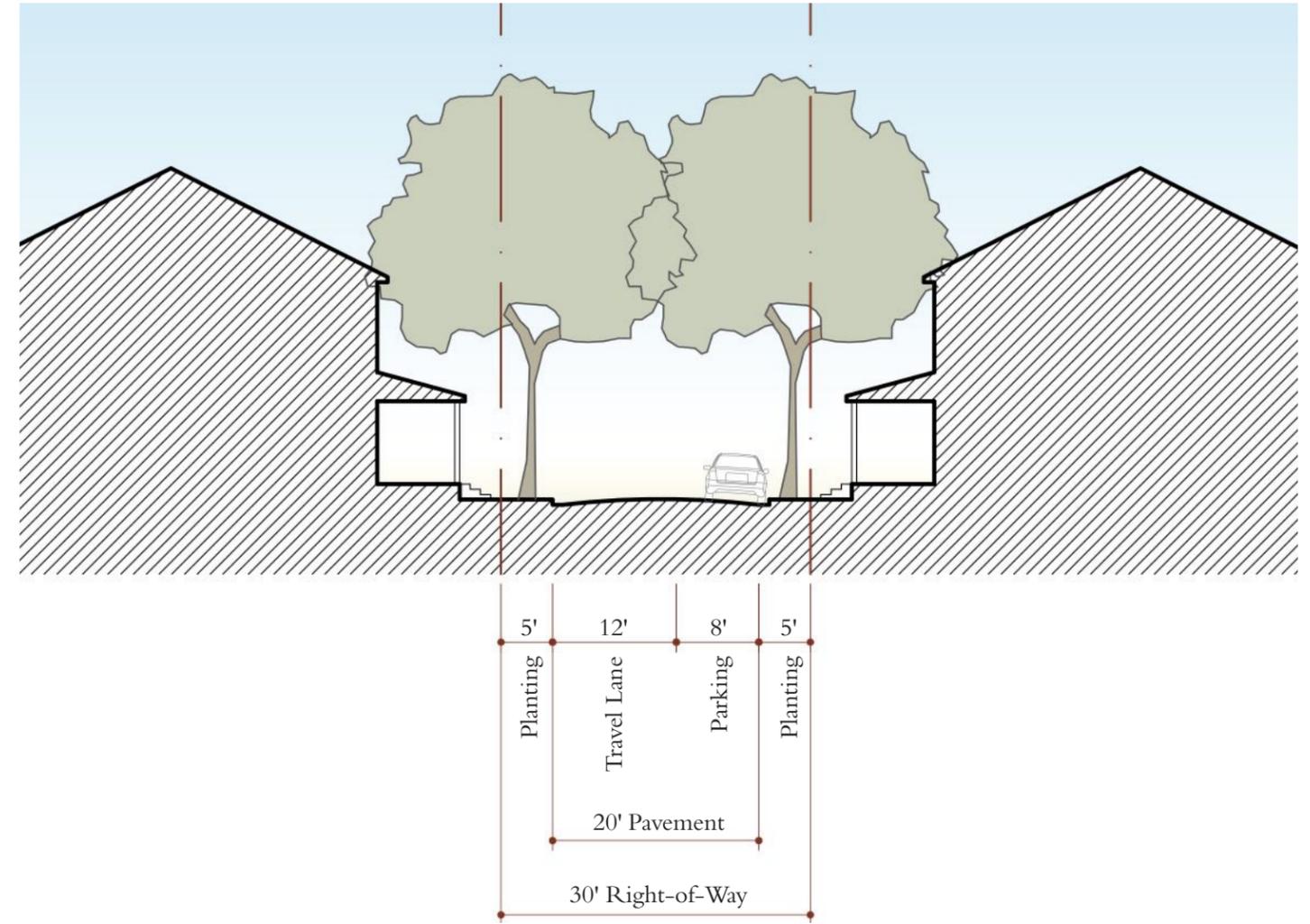
|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Drive  | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | Header                             |
| <b>Vehicular Movement:</b>                       | 2-way  | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 20 ft. |                           |                                    |

**PUBLIC REALM: THOROUGHFARE TYPES**



**DR-63-27**

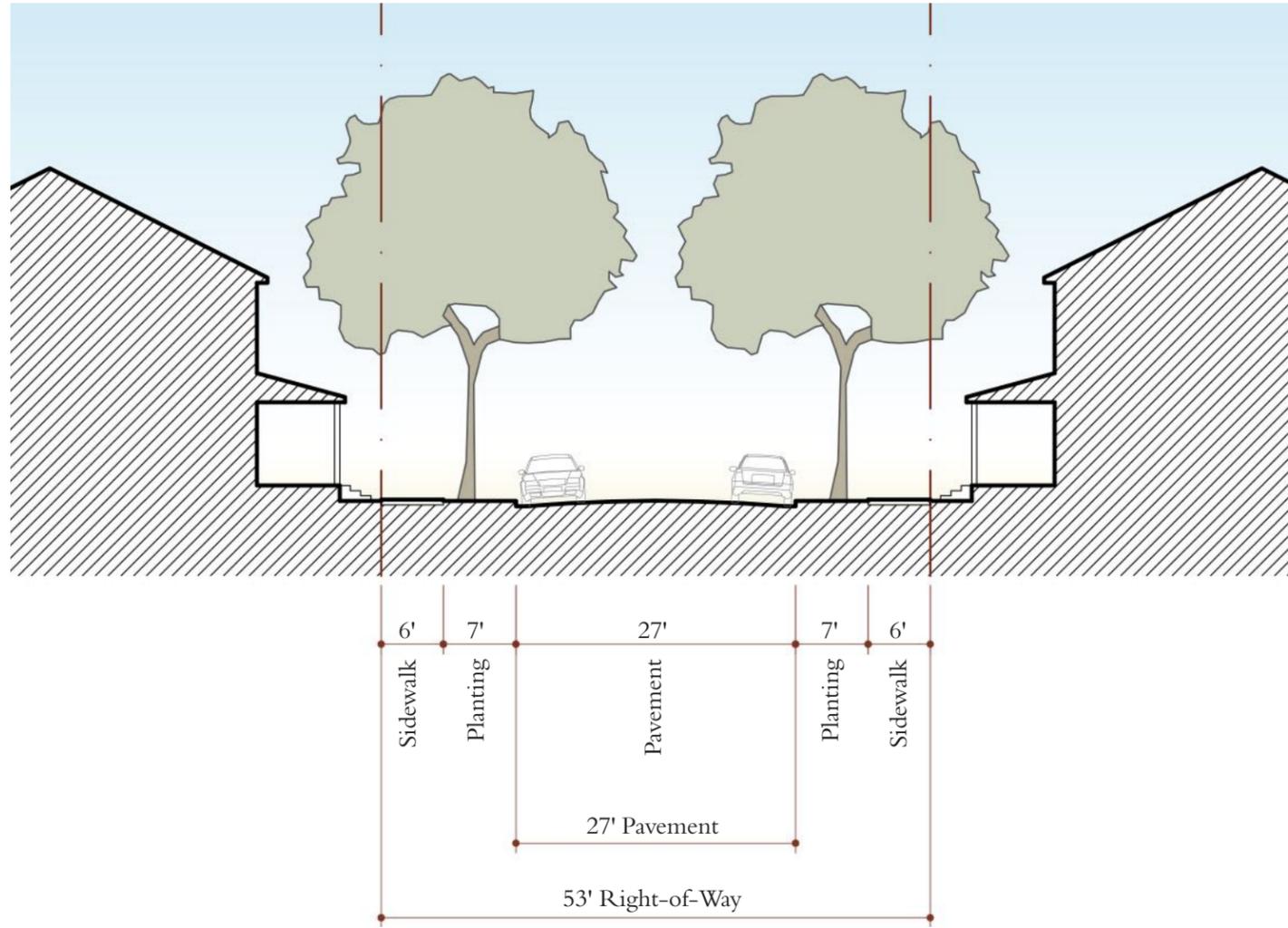
|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Drive  | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | Rolled or Header                   |
| <b>Vehicular Movement:</b>                       | 2-way  | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 20 ft. |                           |                                    |



**LN-30-20**

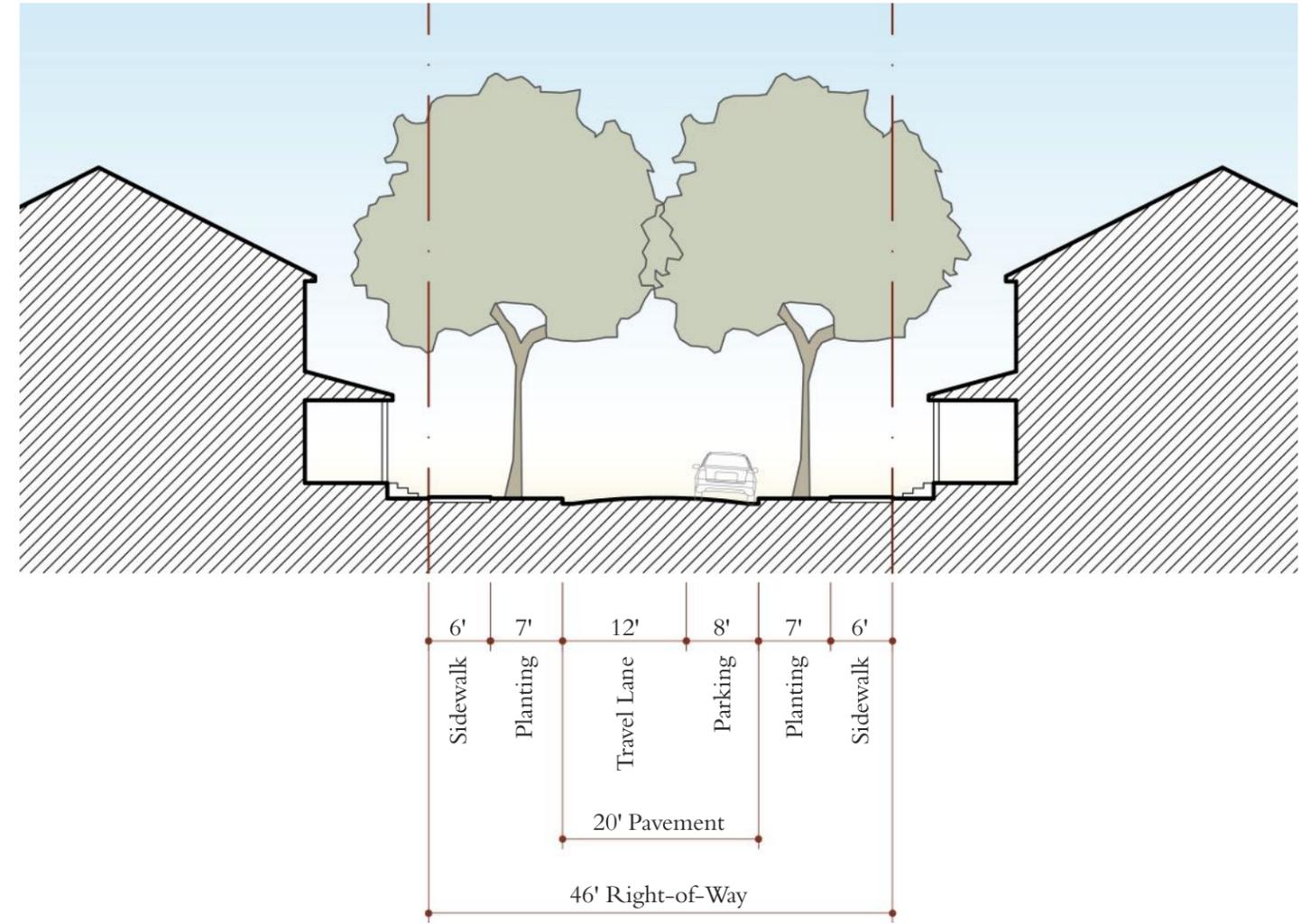
|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Lane   | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | Header                             |
| <b>Vehicular Movement:</b>                       | 2-way  | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 12 ft. |                           |                                    |

**PUBLIC REALM: THOROUGHFARE TYPES**



**YS-53-27**

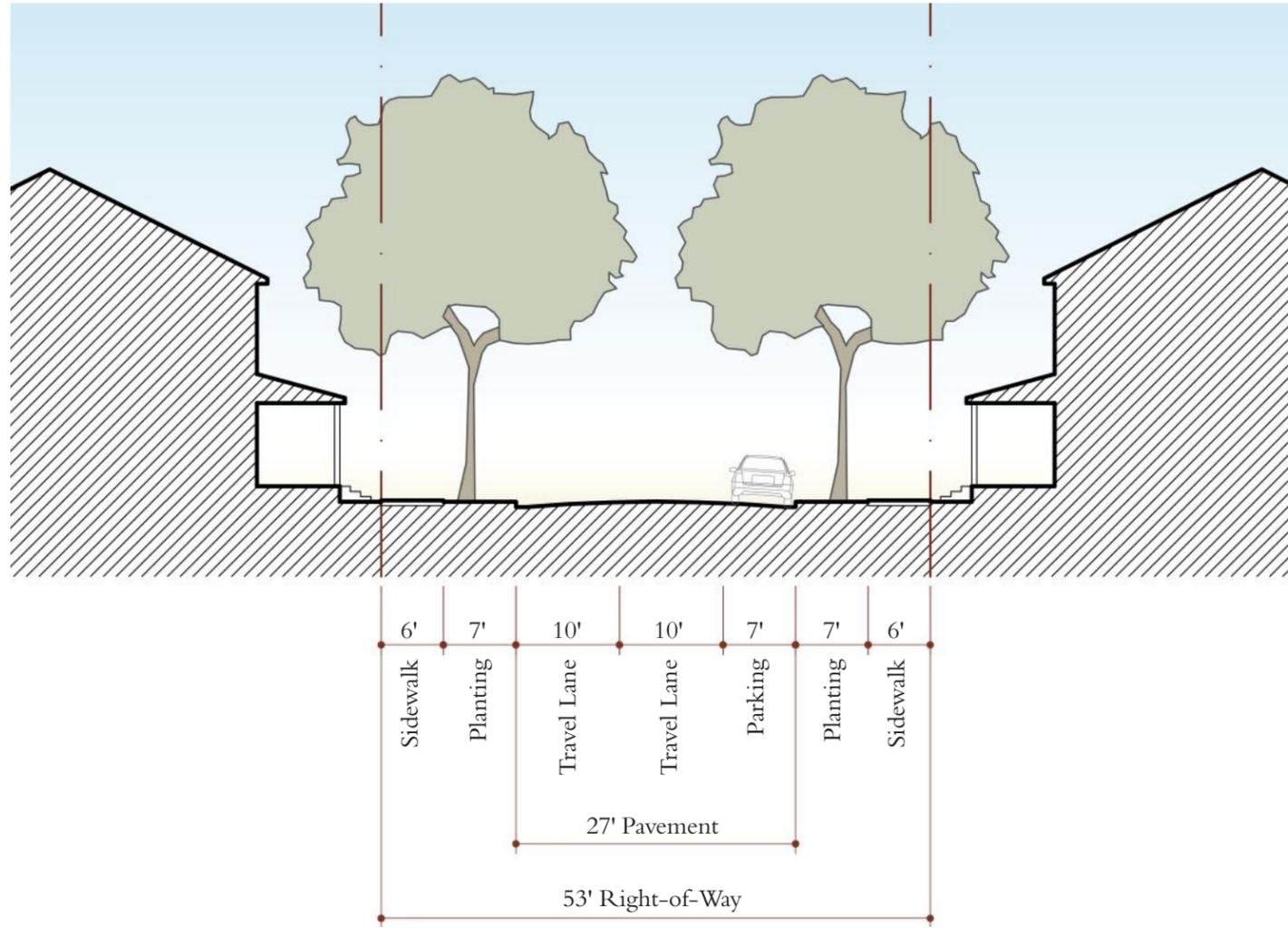
|  |              |                           |                                    |
|--|--------------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Yield Street | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public       | <b>Curb Type:</b>         | Header                             |
| <b>Vehicular Movement:</b>                       | 2-way Yield  | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph       | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | Varies       |                           |                                    |



**ST-46-20**

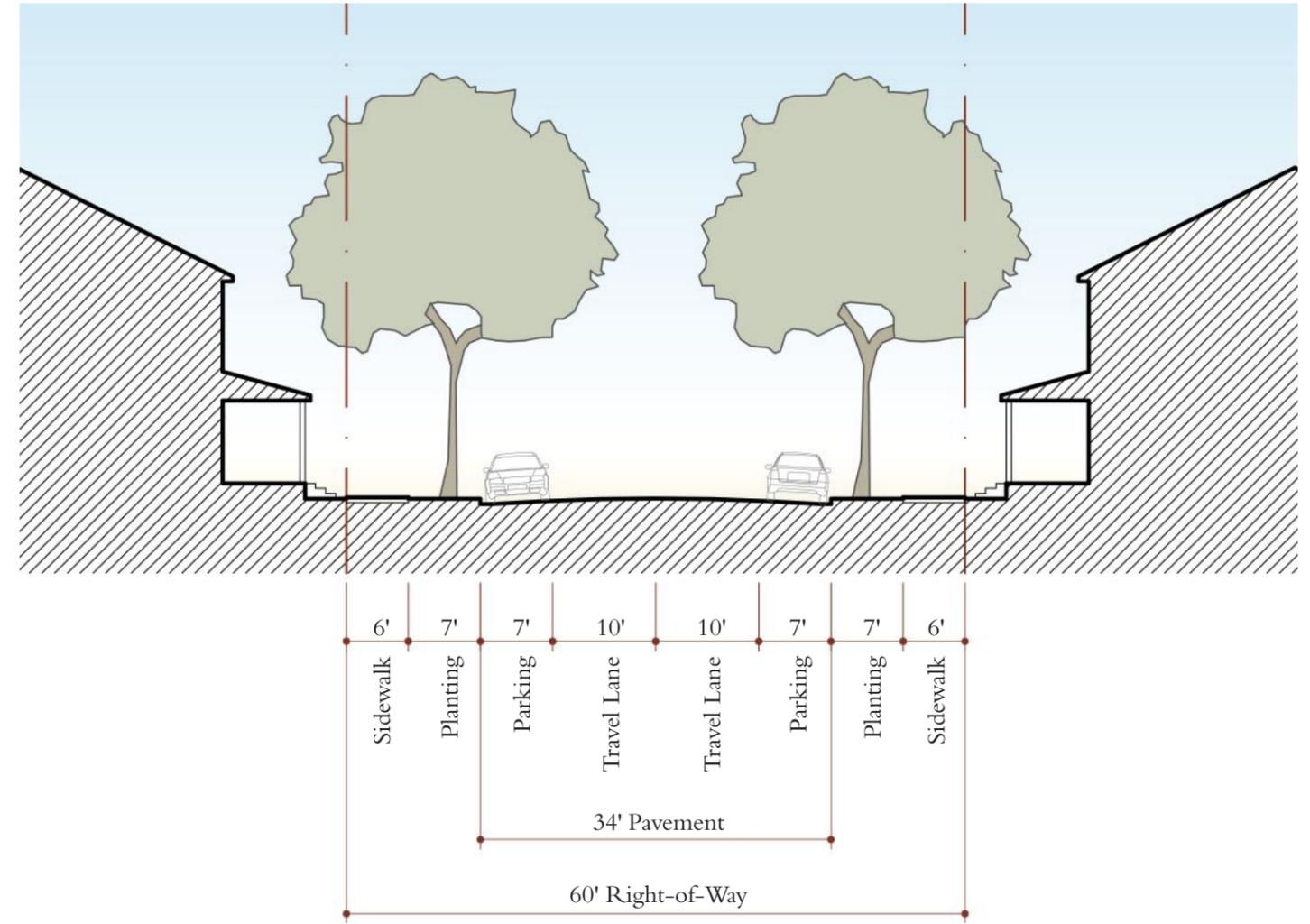
|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Street | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | Header                             |
| <b>Vehicular Movement:</b>                       | 1-way  | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 20 ft. |                           |                                    |

**PUBLIC REALM: THOROUGHFARE TYPES**



**ST-53-27**

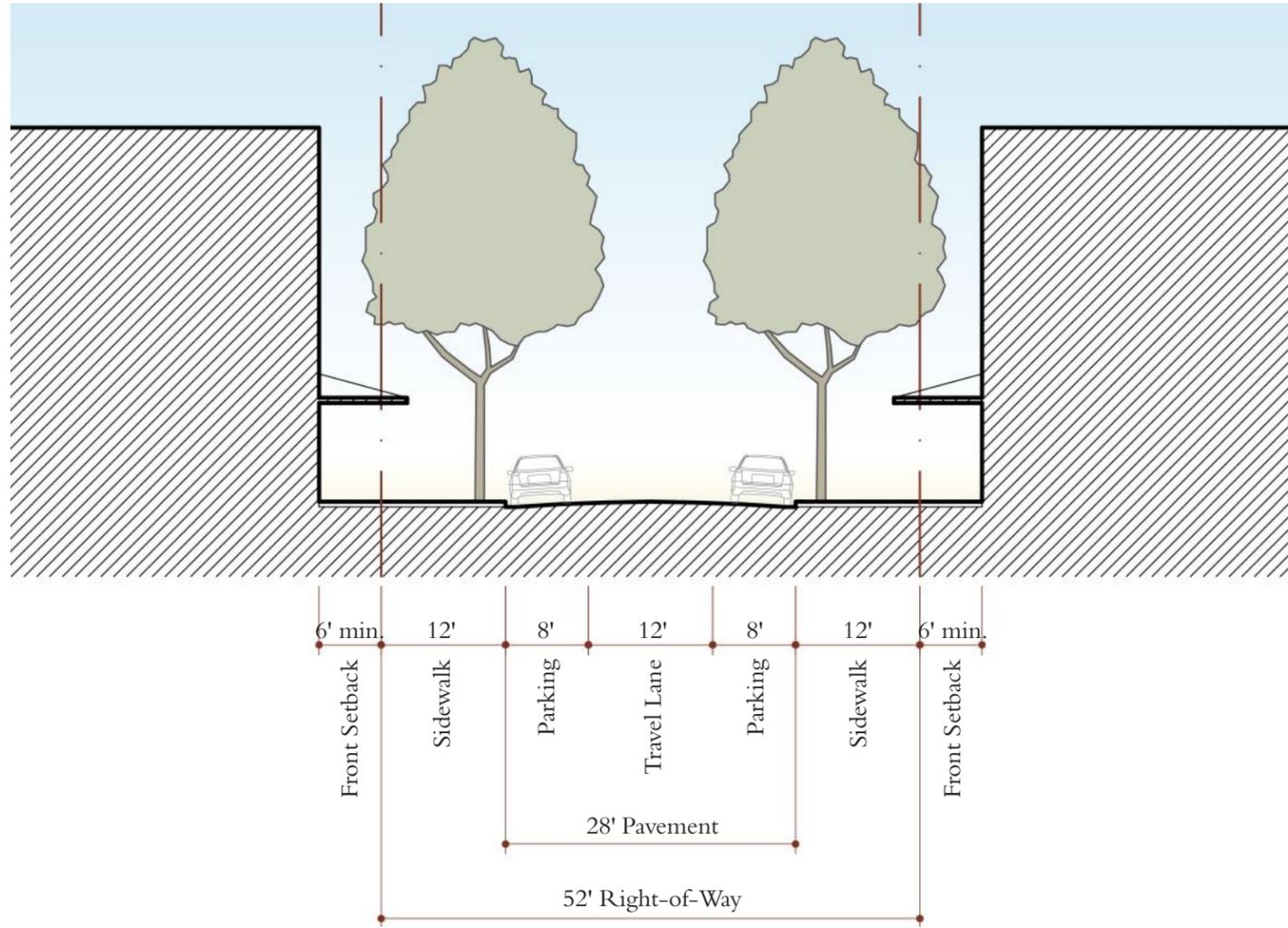
|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Street | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | Header                             |
| <b>Vehicular Movement:</b>                       | 2-way  | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 20 ft. |                           |                                    |



**ST-60-34**

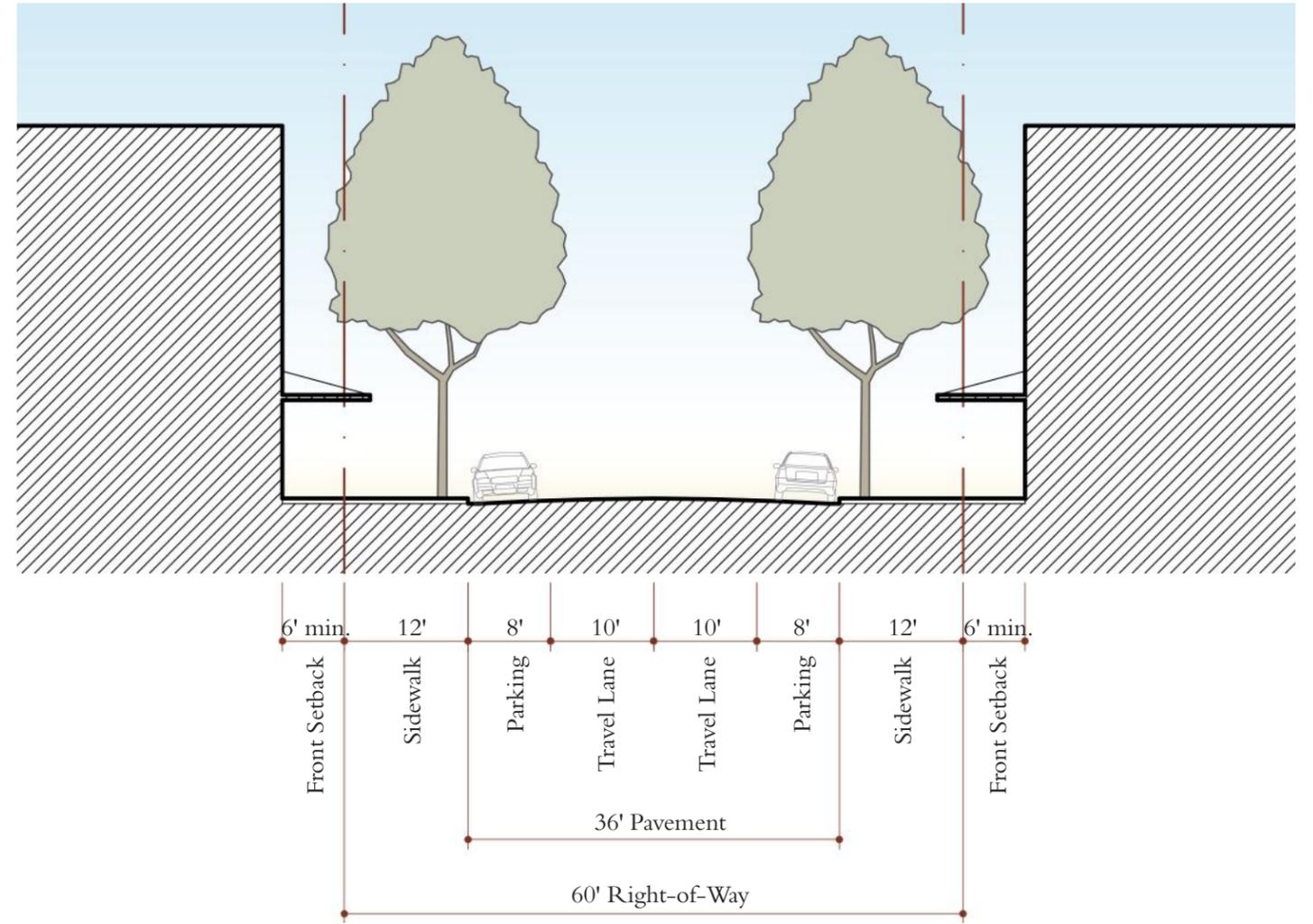
|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Street | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | Header                             |
| <b>Vehicular Movement:</b>                       | 2-way  | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 20 ft. |                           |                                    |

**PUBLIC REALM: THOROUGHFARE TYPES**



**CS-52-28**

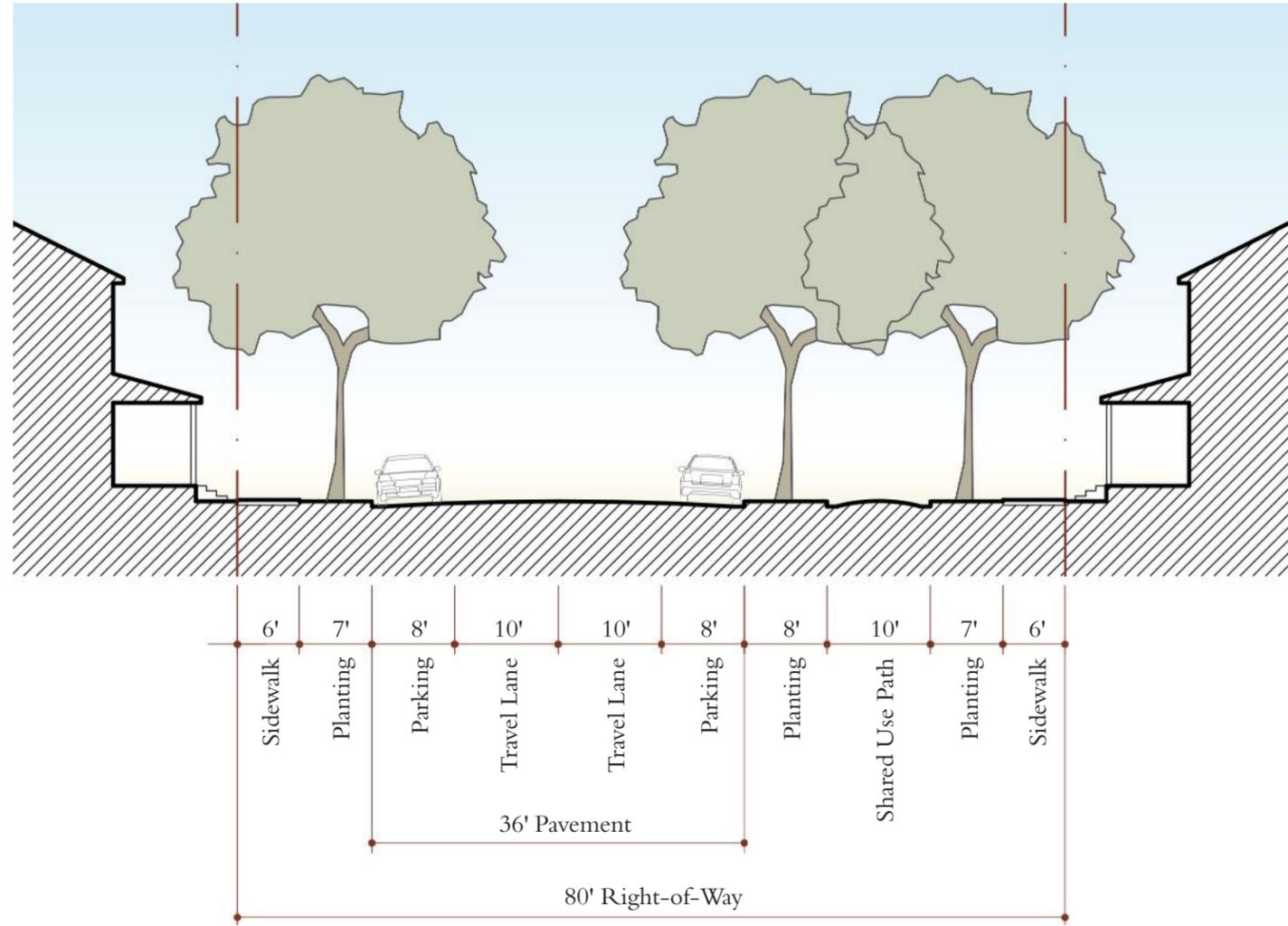
|  |                   |                           |                                    |
|--|-------------------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Commercial Street | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public            | <b>Curb Type:</b>         | Header                             |
| <b>Vehicular Movement:</b>                       | 1-way             | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph            | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 12 ft.            |                           |                                    |



**CS-60-36**

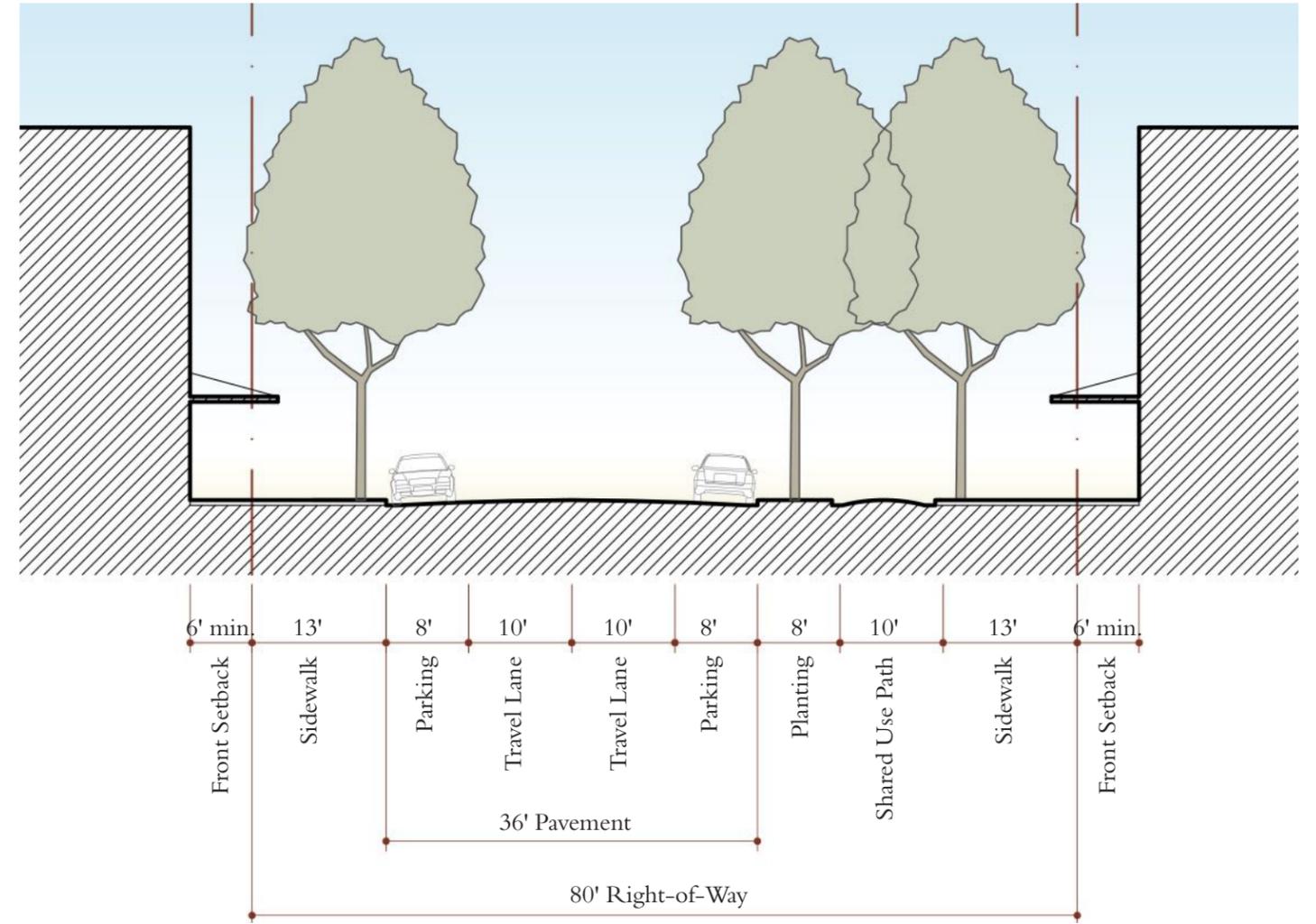
|  |                   |                           |                                    |
|--|-------------------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Commercial Street | <b>Bicycle Facility:</b>  | Shared with Vehicles               |
| <b>Ownership:</b>                                | Public            | <b>Curb Type:</b>         | Header                             |
| <b>Vehicular Movement:</b>                       | 2-way             | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph            | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 20 ft.            |                           |                                    |

**PUBLIC REALM: THOROUGHFARE TYPES**



**AV-80-36**

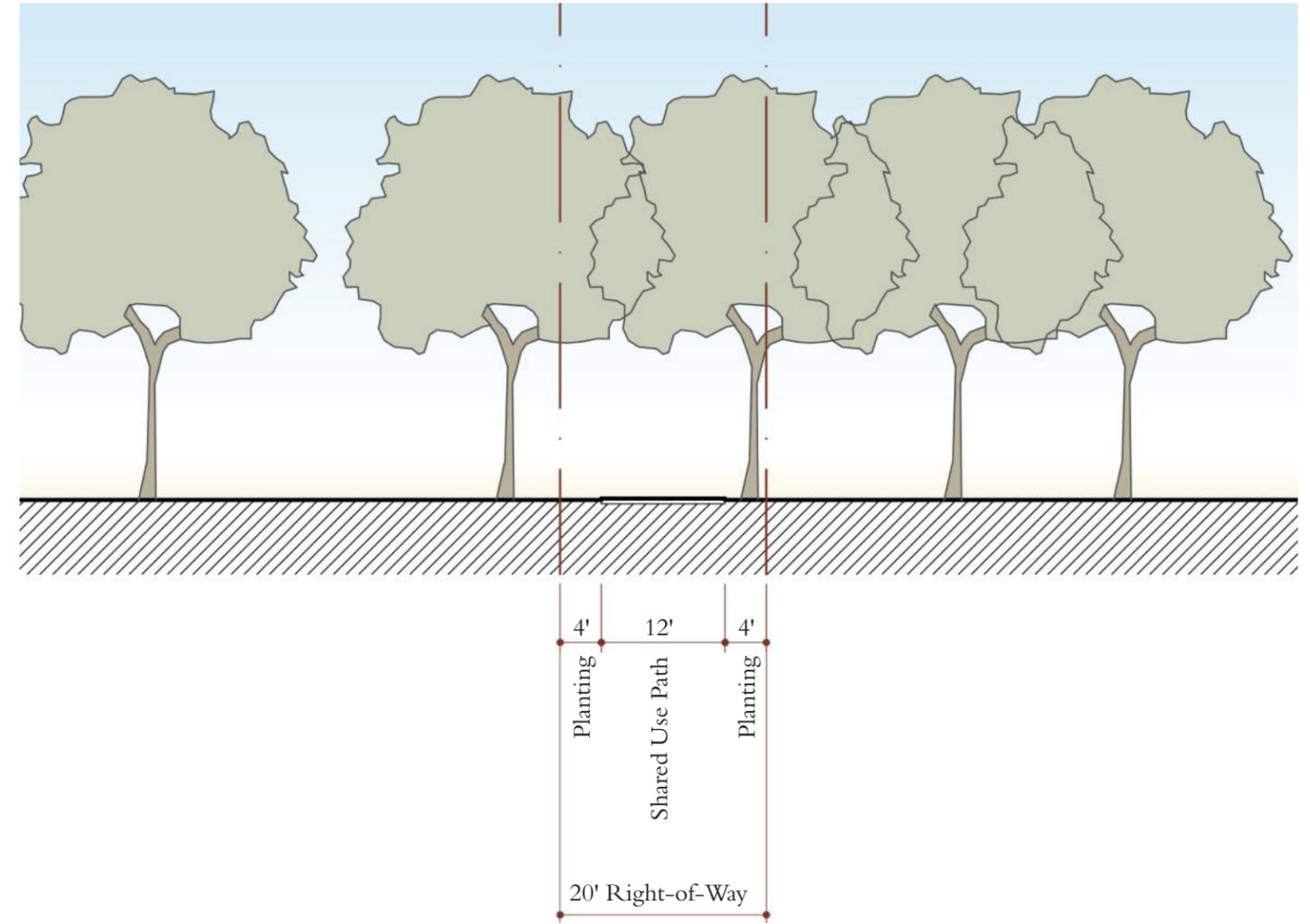
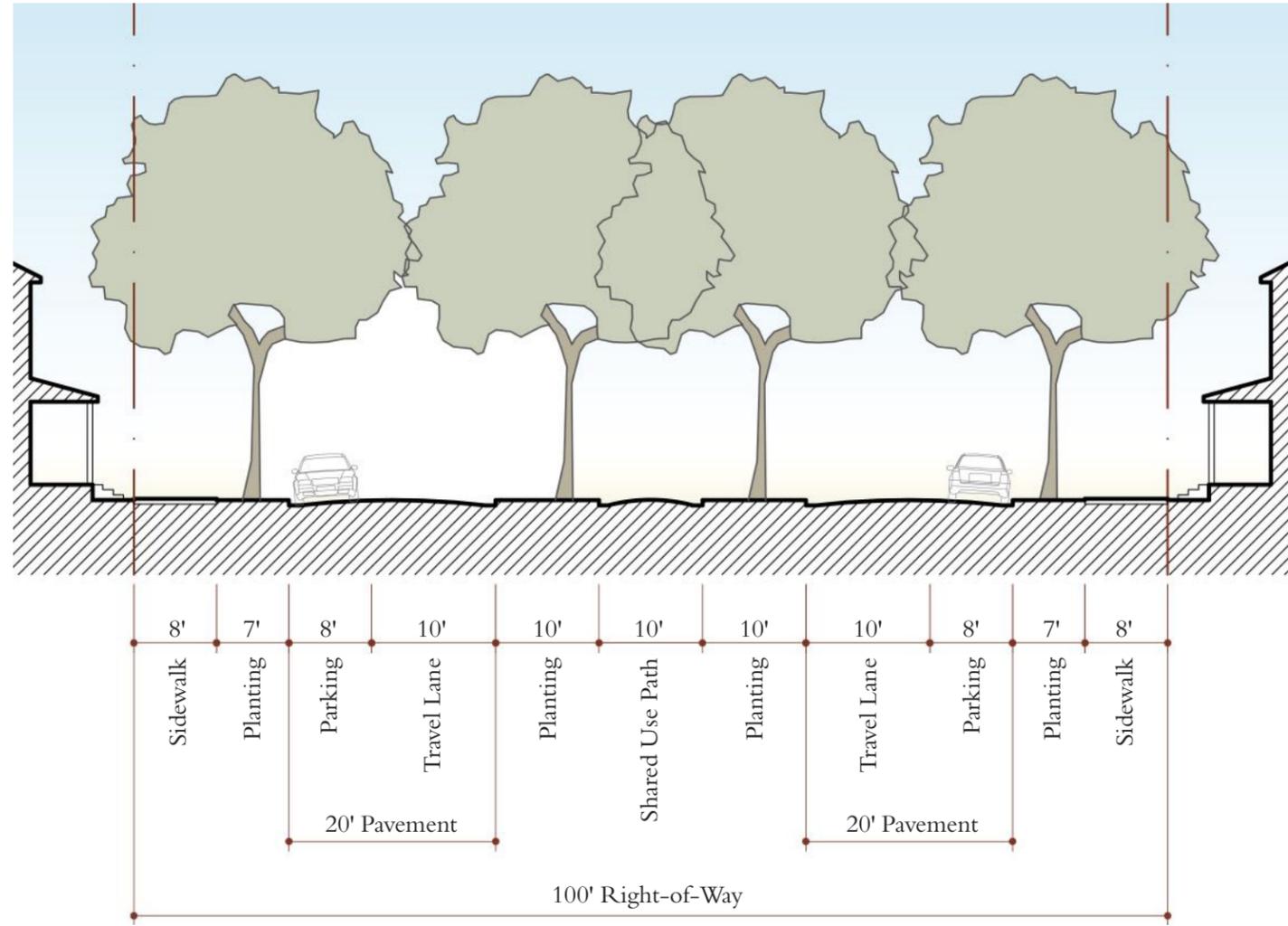
|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Avenue | <b>Bicycle Facility:</b>  | Shared Use Path                    |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | Header                             |
| <b>Vehicular Movement:</b>                       | 2-way  | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 20 ft. |                           |                                    |



**AV-80-36-C**

|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Avenue | <b>Bicycle Facility:</b>  | Shared Use Path                    |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | Header                             |
| <b>Vehicular Movement:</b>                       | 2-way  | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 20 ft. |                           |                                    |

**PUBLIC REALM: THOROUGHFARE TYPES**



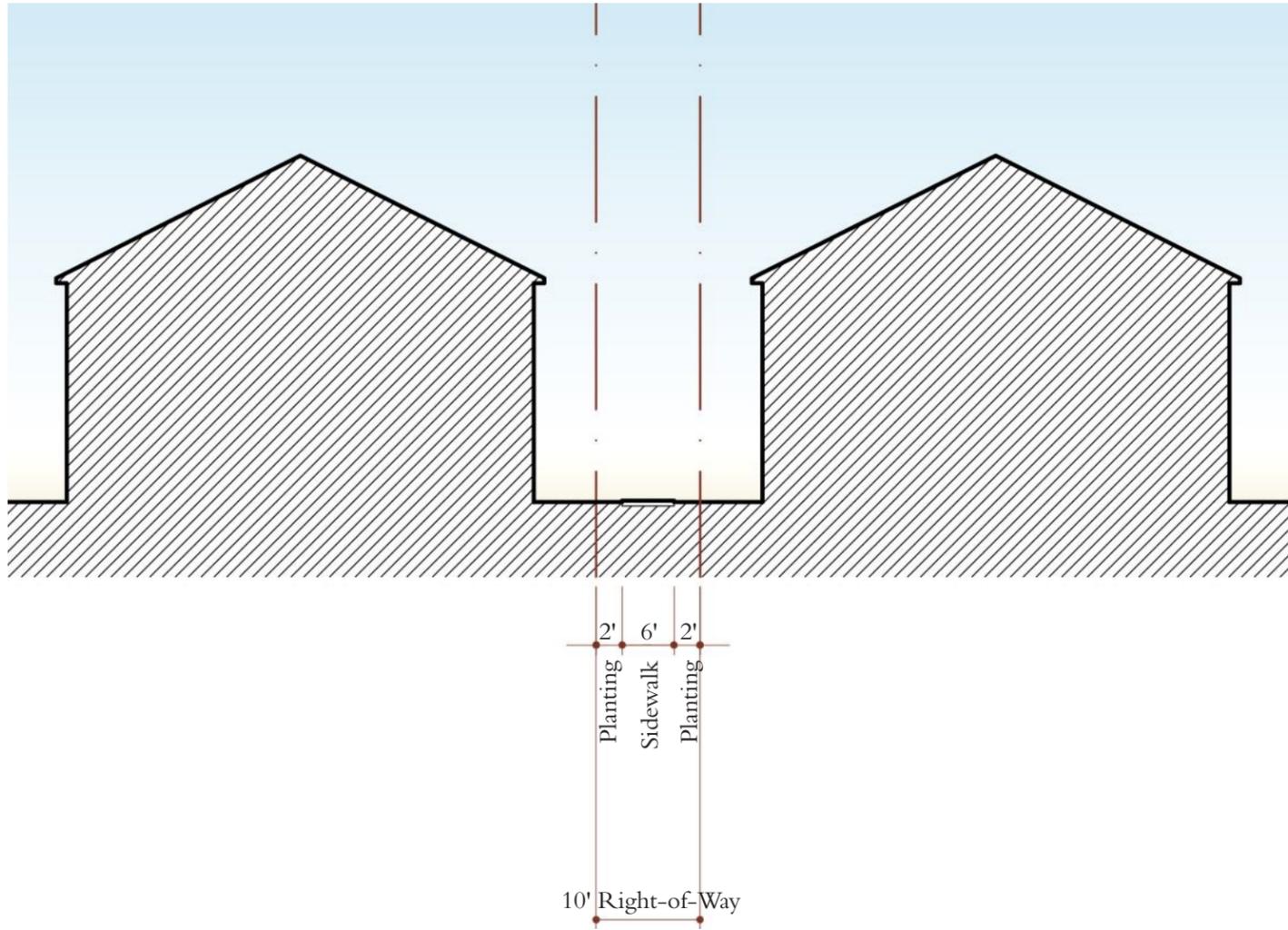
**BV-100-20-20**

|  |           |                           |                                    |
|--|-----------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Boulevard | <b>Bicycle Facility:</b>  | Shared Use Path                    |
| <b>Ownership:</b>                                | Public    | <b>Curb Type:</b>         | Header                             |
| <b>Vehicular Movement:</b>                       | 2-way     | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | 20 mph    | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 12 ft.    |                           |                                    |

**TR-20**

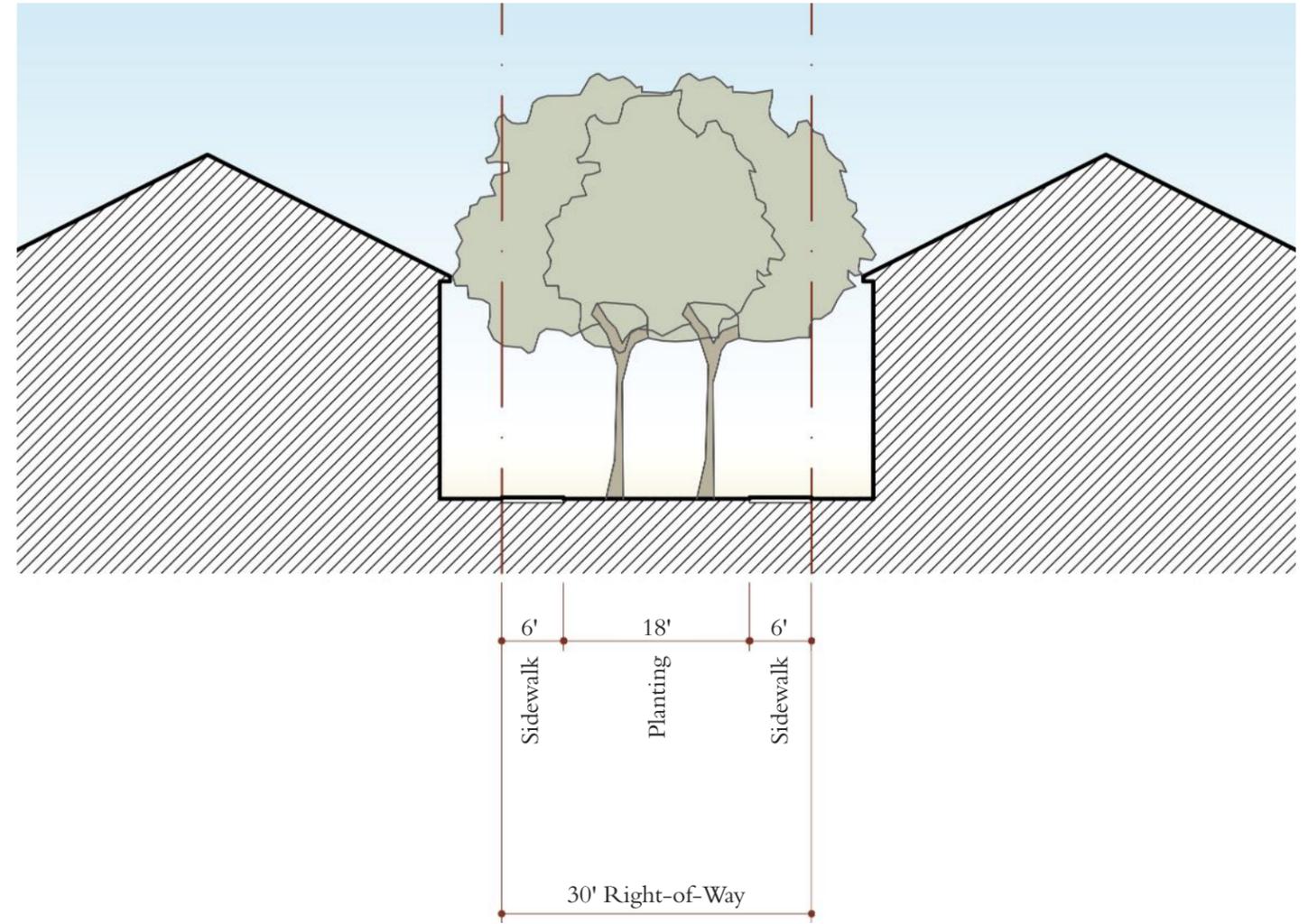
|  |        |                           |   |
|--|--------|---------------------------|---|
| <b>Thoroughfare Type:</b>                        | Street | <b>Bicycle Facility:</b>  | Shared Use Path   |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | None  |
| <b>Vehicular Movement:</b>                       | N/A    | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water  |
| <b>Design Speed:</b>                             | N/A    | <b>Notes:</b>             | This Thoroughfare Type is typically located in Civic Spaces and planting may be located within the ROW or the adjacent Civic Space. |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | N/A    |                           |   |

**PUBLIC REALM: THOROUGHFARE TYPES**



**PT-10**

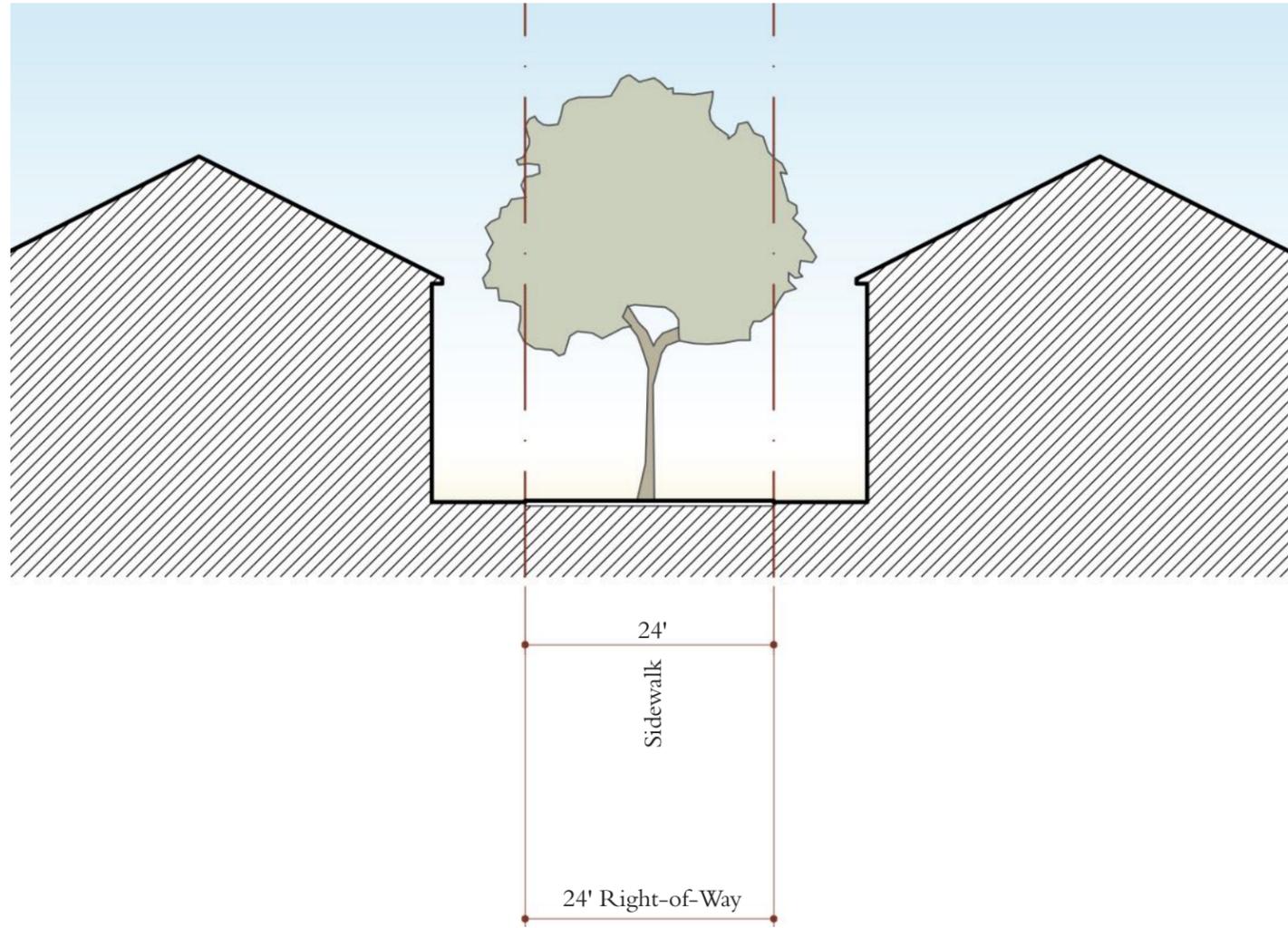
|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Path   | <b>Bicycle Facility:</b>  | None                               |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | None                               |
| <b>Vehicular Movement:</b>                       | N/A    | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | N/A    | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | N/A    |                           |                                    |



**PT-30**

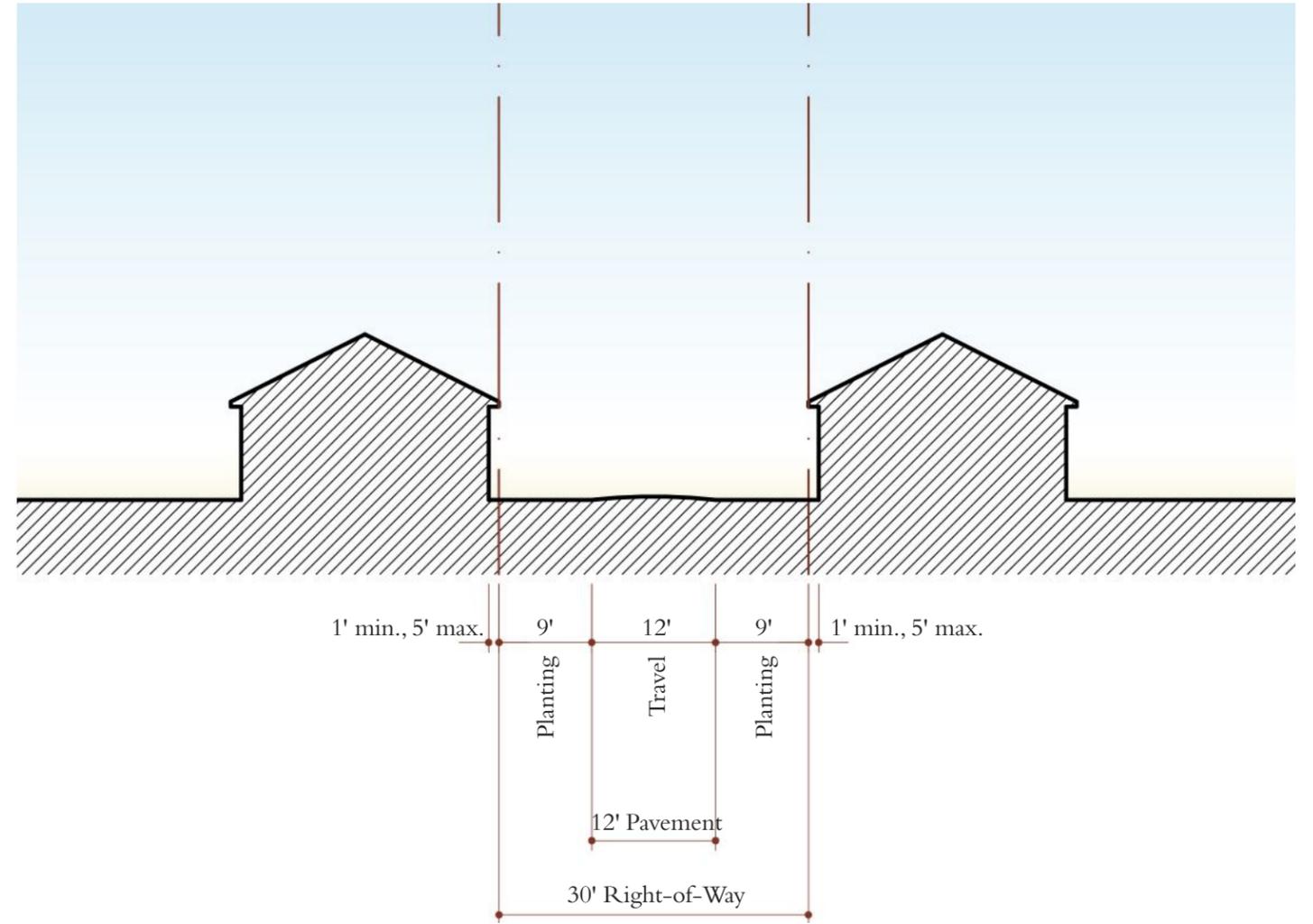
|  |        |                           |                                    |
|--|--------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Path   | <b>Bicycle Facility:</b>  | None                               |
| <b>Ownership:</b>                                | Public | <b>Curb Type:</b>         | None                               |
| <b>Vehicular Movement:</b>                       | N/A    | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | N/A    | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | N/A    |                           |                                    |

**PUBLIC REALM: THOROUGHFARE TYPES**



**PS-24**

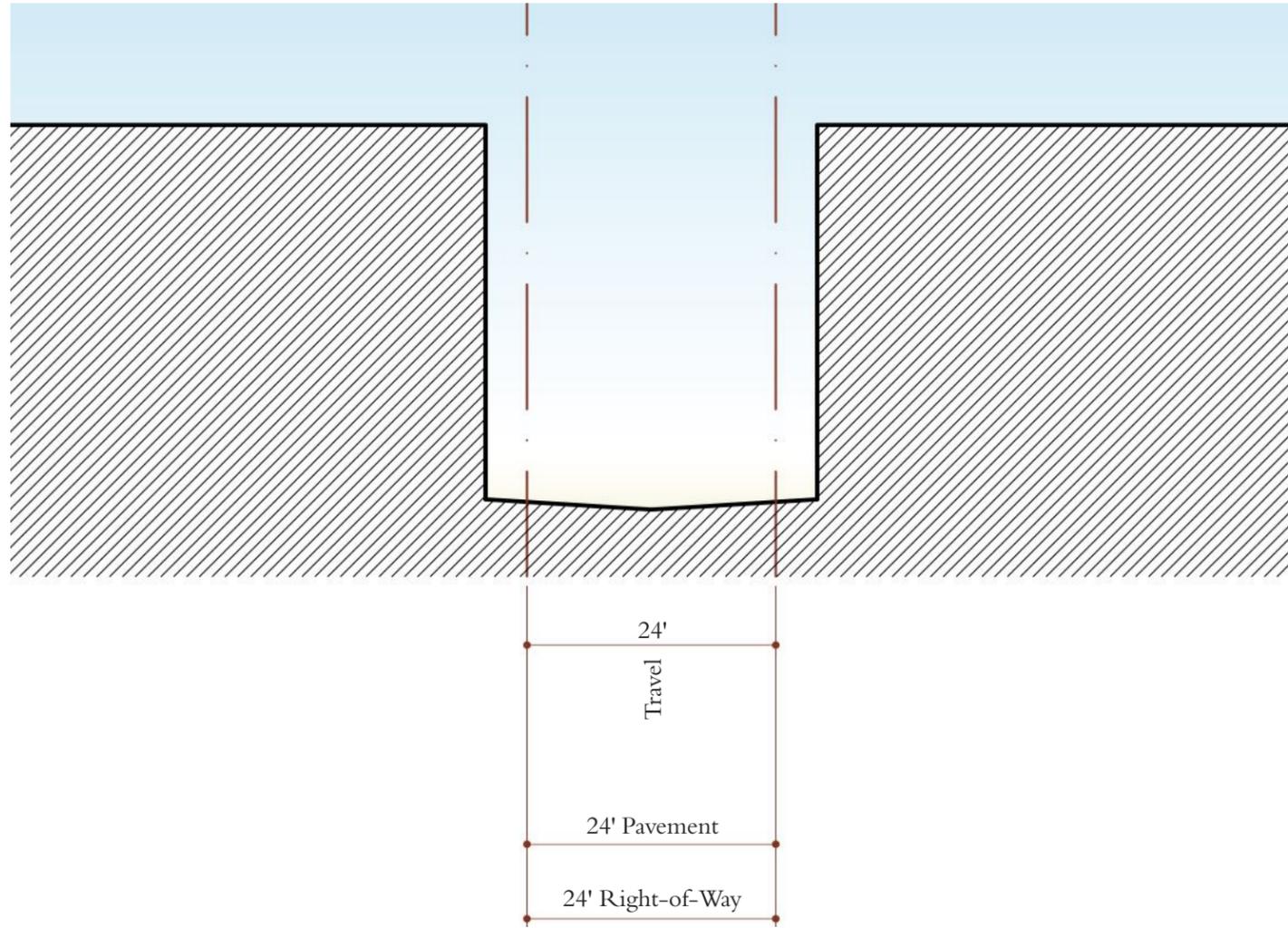
|  |         |                           |                                    |
|--|---------|---------------------------|------------------------------------|
| <b>Thoroughfare Type:</b>                        | Passage | <b>Bicycle Facility:</b>  | Shared with Pedestrians            |
| <b>Ownership:</b>                                | Public  | <b>Curb Type:</b>         | None                               |
| <b>Vehicular Movement:</b>                       | N/A     | <b>Typical Utilities:</b> | Sanitary Sewer, Storm Sewer, Water |
| <b>Design Speed:</b>                             | N/A     | <b>Notes:</b>             |                                    |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | N/A     |                           |                                    |



**RA-30-12**

|  |         |                           |                      |
|--|---------|---------------------------|----------------------|
| <b>Thoroughfare Type:</b>                        | Alley   | <b>Bicycle Facility:</b>  | Shared with Vehicles |
| <b>Ownership:</b>                                | Private | <b>Curb Type:</b>         | None                 |
| <b>Vehicular Movement:</b>                       | 2-way   | <b>Typical Utilities:</b> | Data, Gas, Electric  |
| <b>Design Speed:</b>                             | 15 mph  | <b>Notes:</b>             |                      |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 12 ft.  |                           |                      |

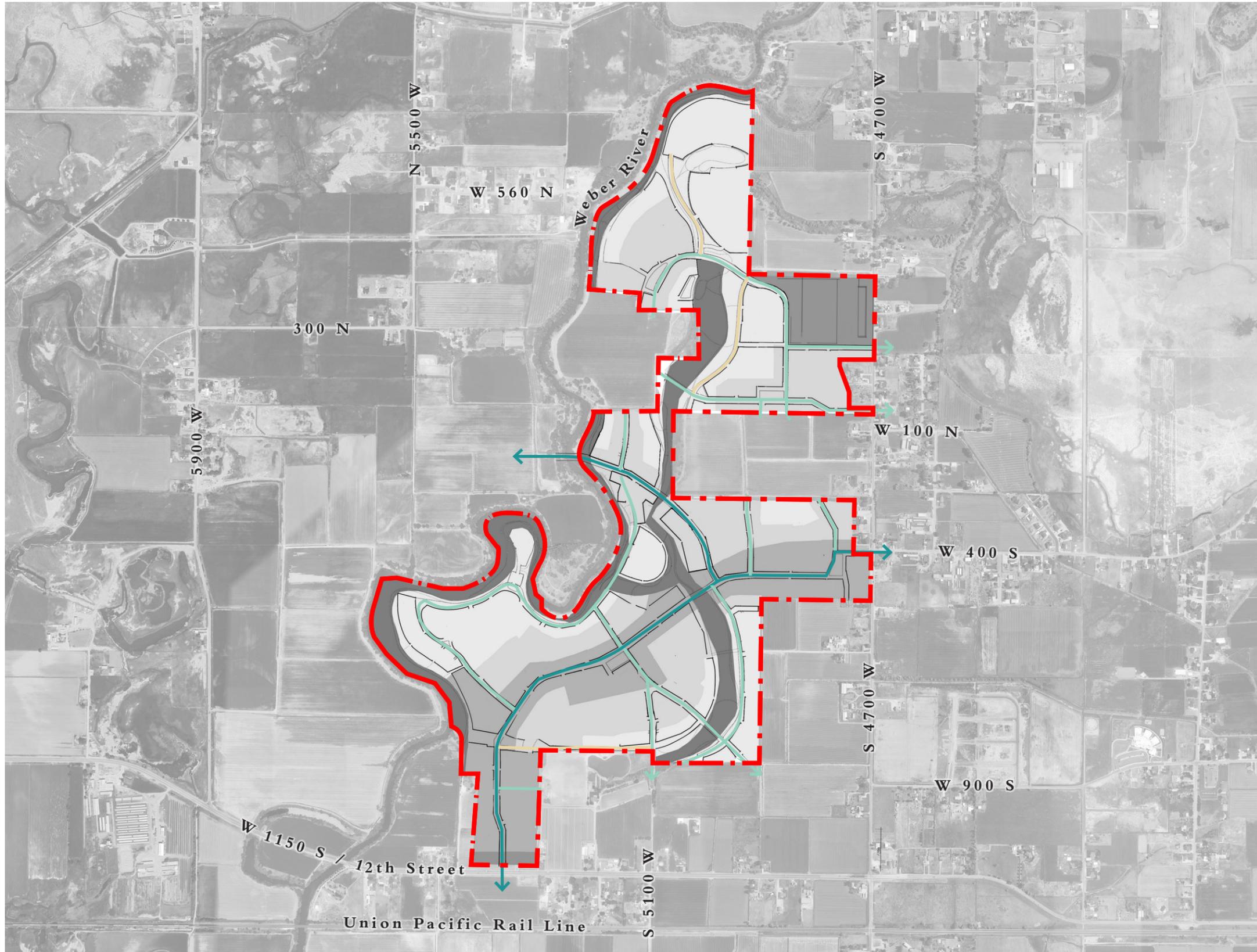
**PUBLIC REALM: THOROUGHFARE TYPES**



· CA-24-24 ·

|  |                  |                           |                      |
|--|------------------|---------------------------|----------------------|
| <b>Thoroughfare Type:</b>                        | Commercial Alley | <b>Bicycle Facility:</b>  | Shared with Vehicles |
| <b>Ownership:</b>                                | Private          | <b>Curb Type:</b>         | None                 |
| <b>Vehicular Movement:</b>                       | 2-way            | <b>Typical Utilities:</b> | Data, Gas, Electric  |
| <b>Design Speed:</b>                             | 15 mph           | <b>Notes:</b>             |                      |
| <b>Vehicular and Fire Apparatus Clear Width:</b> | 24 ft.           |                           |                      |

**PUBLIC REALM: TRANSPORTATION PLAN DIAGRAM**



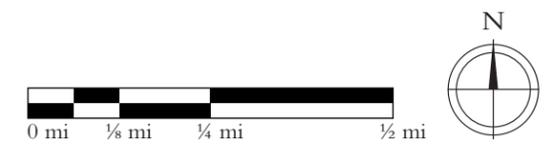
**Key**

- Minor Collector Street
- Major Neighborhood Street
- Minor Neighborhood Street

**Notes**

1. Additional Thoroughfares added will typically be designated as Minor Neighborhood Streets.

*The design is subject to change. Not for construction.*

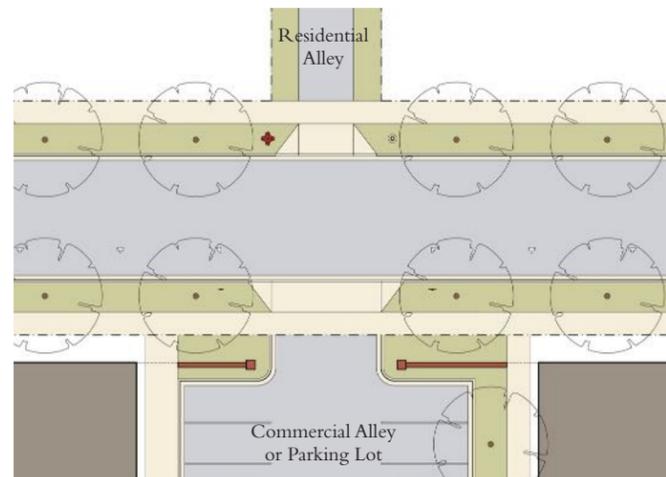


Thoroughfares

1. **Thoroughfare Types.** Each Thoroughfare is assigned a Thoroughfare Type on the Thoroughfare Types Plan (page 9). Each Thoroughfare shall be improved based on its corresponding Thoroughfare Type (pages 10-20) including modifications based on the specific conditions of the Site and the Master Plan.
2. **Ownership.** Thoroughfares shall be publicly owned unless noted otherwise in the Thoroughfare Type Sections (pages 10-20).
3. **Thoroughfare Uses & Encroachments.** The following uses are permitted within Thoroughfares as consistent with the Encroachments Table (page 28) in this Design Code.
  - a. Sidewalk dining.
  - b. Retail, including restaurants.
  - c. Art installations.
  - d. Recreation facilities.
  - e. Community gardens.
  - f. Outdoor festivals or gatherings.
4. **Roadway Geometry.** The following standards apply to the design of all Vehicular Thoroughfares in the Master Plan, with the intent of calming traffic and creating a safe, comfortable space for pedestrians and cyclists:
  - a. **Roadway Width.** Pavement and vehicular lane widths shall be measured from face-of-curb to face-of-curb and include the width of adjacent gutter pans, if any.
  - b. **Turning Radius.** Turning radii shall be evaluated using Effective Turning Radius.
  - c. **Centerline Crossing.** Emergency and service vehicles may cross the centerline to complete turning movements.
  - d. **Centerline Radius.** The centerline radius for roadways shall be a minimum of 22 feet.
  - e. **Sight Visibility Triangles.** The outside edges of Vehicular Travel Lanes, not Thoroughfares or Roadways, shall be used in the determination of sight visibility triangles that control the placement of potential obstructions. Groundcover other than turf grass is permitted within sight visibility triangles.

- f. **Chamfered & Radiused Lots.** The area required for sight visibility triangles shall typically be contained within the width of the Thoroughfare through the use of Parking Lanes, Planting Strips, Sidewalks, and other Thoroughfare Components wherever reasonably possible. Lot Lines shall only be chamfered or radiused if necessary to satisfy requirements related to sight visibility triangles.
5. **Intersections.** The following requirements apply to the design of all intersections of Vehicular Thoroughfares in the Master Plan, with the intent of calming traffic and creating a safe, comfortable space for pedestrians and cyclists.
  - a. **Intersection Spacing.** Thoroughfares shall not be subject to intersection spacing requirements.
  - b. **Intersection Angles.** Intersection angles and offsets are permitted if consistent with the Master Plan or minor adjustments therein. Thoroughfares may have offset intersections or intersections at angles other than 90 degrees as a means of calming traffic.
  - c. **Alley Intersections.** Alley intersections shall conform to the design shown in the Alley Intersection Diagram or another design consistent with the intent of this section

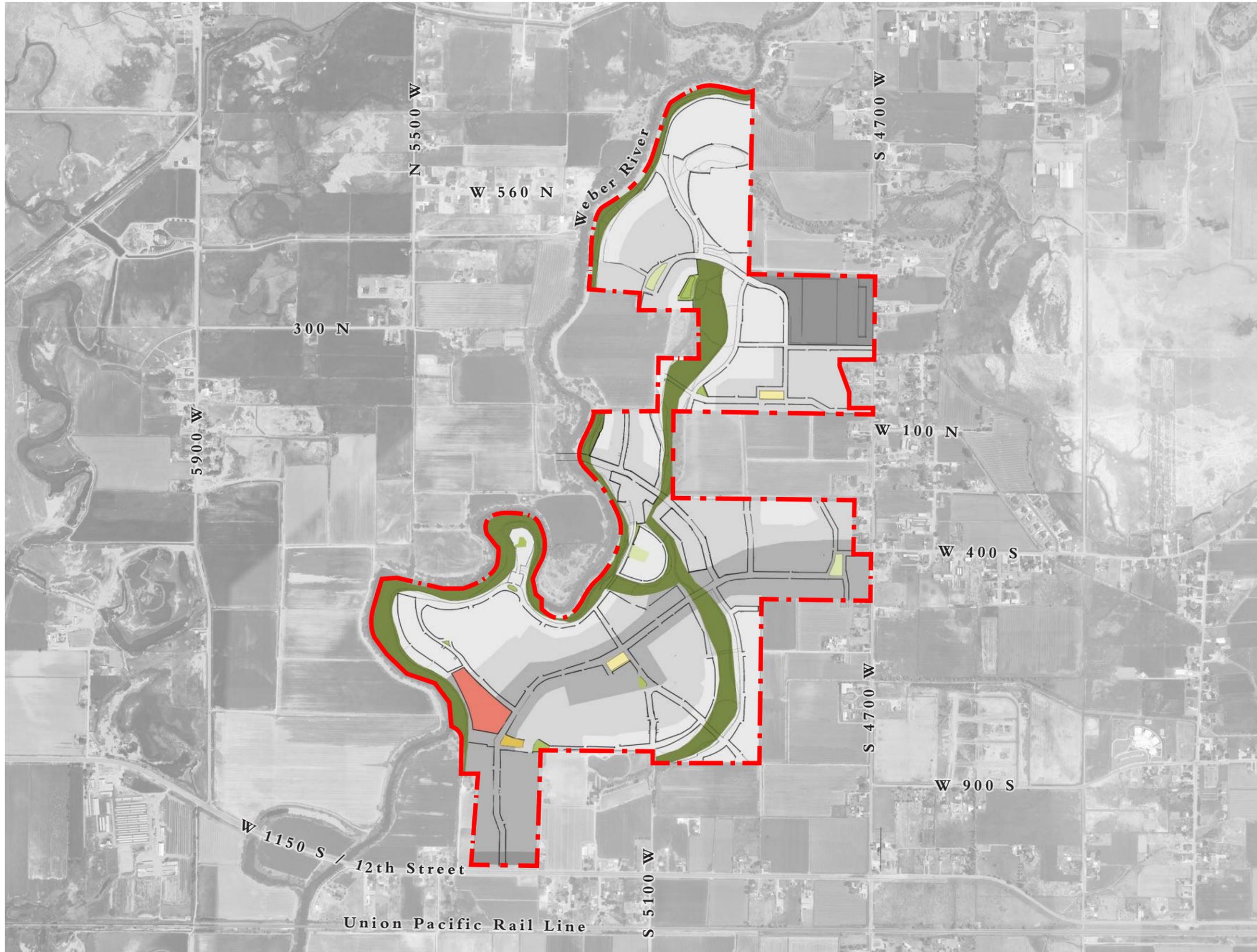
Street & Alley Intersection Diagram



6. **Additional Thoroughfare Types.** Additional Thoroughfare Types that consist of a combination of the components described in this Design Code may be incorporated into refinements of the Master Plan in later submissions.
7. **Vehicular Travel Lanes.** The following apply to the design of vehicular travel lanes.
  - a. **Configuration.** The travel lanes of a thoroughfare (considered together) may be designed to accommodate one-way, two-way, or two-way yield movement.
  - b. **Fire Apparatus Access.** Travel lanes may be wider than those shown in the Thoroughfare Types in order to accommodate wider clear width for fire apparatus access. This arrangement is subject to the approval of the Fire Official.
8. **Parking Lanes.** The following apply to the design of parking lanes:
  - a. **Configuration.** Parking lanes shall typically consist of parallel parking spaces.
  - b. **Width.** Parallel parking lanes shall typically be 7 feet wide on predominantly Residential Thoroughfares and 8 feet wide on predominantly Commercial Thoroughfares.
  - c. **Fire Apparatus Access.** Parking spaces or portions thereof may be omitted from parking lanes in order to accommodate wider clear width for fire apparatus access.
9. **Medians.** Medians have a minimum typical width of 18 feet, which may taper to no less than 8 feet based on the specific conditions of the Site and the Master Plan. Medians shall comply with the landscaping requirements for Planting Strips in the Public Realm Details section of this Design Code (page 25). Medians may also include Sidewalks, pedestrian paths, and shared-use paths.
10. **Public Frontage.** Standards for components of the public frontage, including street signs and lights, are listed in the Public Realm Details section of this Design Code (page 25).

11. **Paving Material.** Vehicular travel and parking lanes may be paved with any material or combination of materials listed in the Public Realm Details section of this Design Code (page 25).
12. **Striping.** Striping between any vehicular lanes and between parking spaces on any Thoroughfare may be omitted.

**PUBLIC REALM: CIVIC SPACE PLAN**

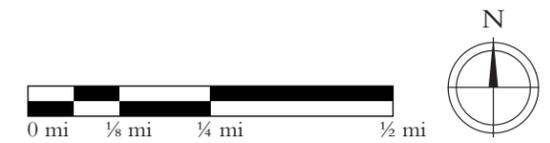


**Key**

- Plaza
- Square
- Green
- Park
- Pocket Park
- Civic Building Site

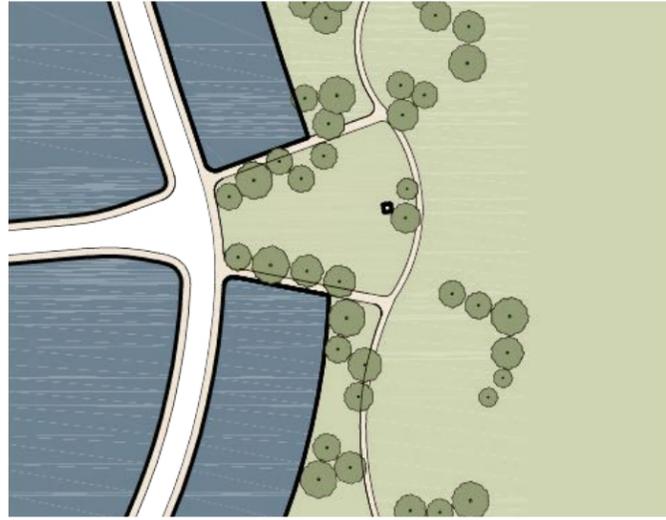
**Notes**

1. Civic Spaces may be added or removed as consistent with this Design Code, and any revisions to the Master Plan shall comply with the minimum Civic Space allocation referenced in the Neighborhood Structure Section of this Design Code (page 7).
2. The Civic Building Site shown is intended for a school, and may be replaced by Blocks of Private Lots if a school is no longer anticipated.



*The design is subject to change. Not for construction.*

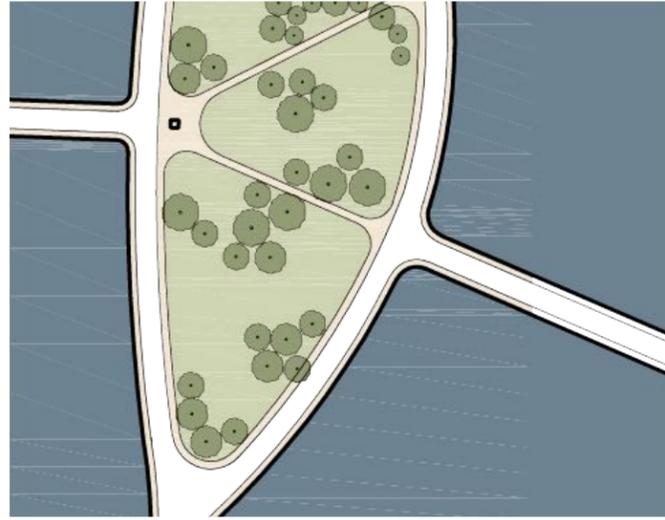
## PUBLIC REALM: CIVIC SPACE TYPES



### Park

**Transect Zones:** T2, T3, T4

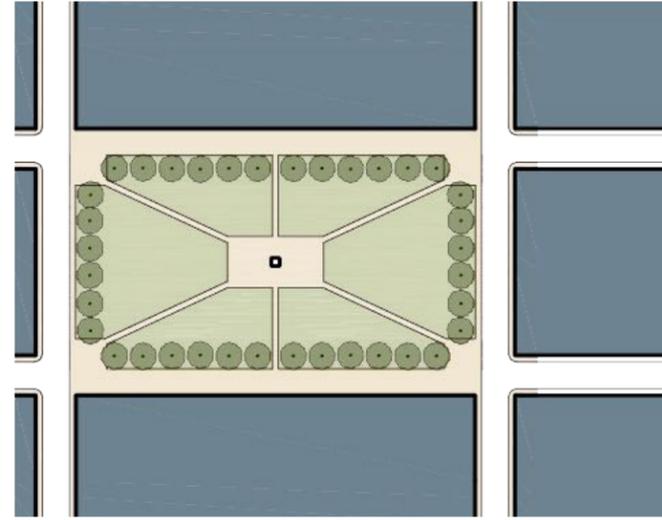
**Intended Character:** A large Civic Space available for recreation, often serving as entire neighborhood or region. Its landscape may include paved paths and unpaved trails, open lawn, trees, ponds, and other natural features, all with a naturalistic character.



### Green

**Transect Zones:** T2, T3, T4, T5

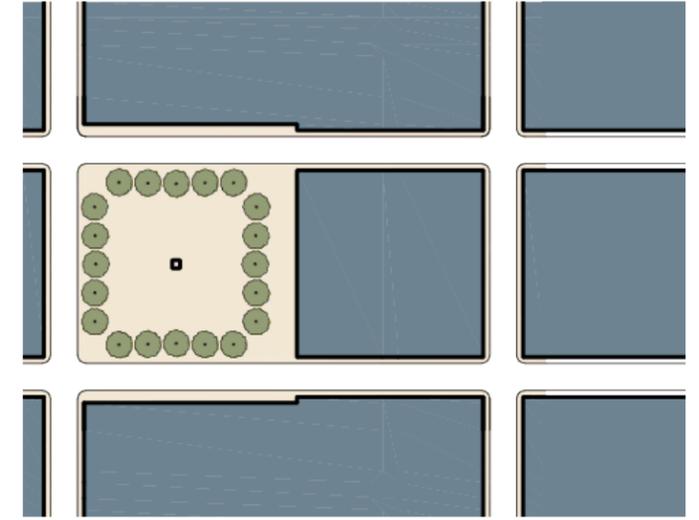
**Intended Character:** A medium Civic Space available for recreation. A green is typically shaped by buildings and its landscape consists mostly of lawn and trees with a naturalistic character requiring limited maintenance.



### Square

**Transect Zones:** T3, T4, T5

**Intended Character:** A small-to-medium Civic Space, typically at the intersection of important thoroughfares. A Square is shaped by buildings, some or all of which may be mixed-use, and its landscape consisted of paved walks, lawn, trees, and Civic Buildings with a formal character.



### Plaza

**Transect Zones:** T4, T5

**Intended Character:** Civic Space which is predominantly paved. A Plaza is often fronted with mixed-use or commercial buildings, as the hardscape is ideal for outdoor seating.

## Civic Spaces

1. **Applicability.** This section applies to all Civic Spaces in the Master Plan.
2. **Civic Space Types.** Each Civic Space is assigned a Civic Space Type on the Civic Space Plan (page 23). Each Civic Space shall be improved based on its corresponding Civic Space Type (page 24), including modifications based on the specific conditions of the Site and the Master Plan.
3. **Civic Space Adjustments.** The location, size, and design of Civic Spaces may be refined or modified based on the specific conditions of the Site and the Master Plan.
4. **Ownership.** Civic Spaces may be publicly or privately owned.
5. **Pedestrian Connectivity & Access to Lots.** Civic Spaces shall have Sidewalks or Pedestrian Paths sufficient to provide access to all Enfronting lots and provide connectivity to adjacent Front Thoroughfares and Civic Spaces. These Sidewalks or pedestrian paths shall comply with the Public Realm Details Section of this Design Code.
6. **Civic Space Uses & Encroachments.** The following Uses are permitted within Civic Spaces as consistent with the Encroachments Table (page 28) in this Design Code.
  - a. Natural landscape.
  - b. Gardens including ornamental and productive gardens.
  - c. Lawns.
  - d. Recreation facilities including sport courts, pools, skate parks, and exercise equipment.
  - e. Playgrounds.
  - f. Stormwater managements facilities.
  - g. Cemeteries.
  - h. Sidewalk dining.
  - i. Retail including restaurants.
  - j. Art installations.
  - k. Outdoor festivals or gatherings.
7. **Stormwater Management.** The area of stormwater management facilities shall count towards the required Civic Spaces if designed in a manner that is integrated into a Civic Space.
8. **Covered Structures.** Covered, unenclosed structures may be included within Civic Spaces and shall count towards the required Civic Spaces. Enclosed buildings in Civic Spaces shall be considered Civic Building Sites.
9. **Landscaping.** Landscaping in Civic Spaces shall consist of ground cover, shrubs, and trees as described in the Public Realm Details section of this Design Code. Minimum required ground planting and quantity of trees are listed in the Civic Space Types (page 24). The minimum required quantity of trees shall be calculated based on the area of the Civic Space.

## Public Realm Details

1. **Sidewalks & Pedestrian Paths.** Sidewalks and pedestrian paths shall have a minimum width of 5 feet and may consist of any paving material specified below (requirement #4). Sidewalks may also include tree grates.
2. **Shared-Use Paths.** Shared-use paths shall have a minimum width of 8 feet and may consist of any paving material specified below (requirement #4).
3. **Planting Strips.** Planting strips shall include ground cover and trees planted in Allees or Clustered. Planting strips of a width less than 5 feet need not include trees. The width of planting strips may be adjusted in order to accommodate curb ramps and adjustments to curb height.
4. **Paving Materials.** In addition to paving materials and surfaces currently acceptable to the County, asphalt, stabilized decomposed granite, limestone, concrete, brick, Grasscrete (and similar products), integral colored concrete, stamped concrete, granite, granite pavers, cobblestone, decomposed granite, tar and chip, asphalt pavers, brick pavers, and concrete pavers may be used. Other materials may also be permitted.
5. **Ground Planting.** Ground planting shall consist of a combination of shrubs and ground cover. Ground cover shall be spaced a maximum of 12 inches apart on center and may include irrigated turf grass.
6. **Tree Planting.** The following requirements apply to tree planting:
  - a. **Total Species Variation.** Individual Thoroughfares and Civic Spaces need not have any variation in tree species.
  - b. **Allees.** Where tree planting is in an Allee, it shall consist of a single species. Trees shall be spaced between 30 and 40 feet apart on center.
  - c. **Clustered.** Where tree planting is clustered, it may consist of multiple species.
  - d. **Distance from Curb & Sidewalk.** The distance of trees from curbs, sidewalks, and pedestrian paths shall be determined by the Town Architect.
7. **Species.** The species used in landscaping for all Thoroughfares and Civic Spaces shall conform to the Approved Plant List and the Prohibited Plant List in the Weber County Code of Ordinances.
8. **Lighting.** The following requirements apply to lighting in Thoroughfares and Civic Spaces:
  - a. **Spacing.** Light fixtures shall be spaced as determined by the Town Architect.
  - b. **Shielding.** All light fixtures shall be fully shielded and direct light downward.
  - c. **Height.** Light pole height shall not exceed 25 feet. The height of wall-mounted light fixtures shall not exceed 30 feet.
  - d. **Light Fixtures.** The design of light fixtures shall be determined by the Town Architect.
  - e. **Signs.** Thoroughfare name and traffic control signs may be attached to street light fixtures.
9. **Furniture.** The following may be located in any Civic Space or in the Public Frontage of any Thoroughfare in a manner that maintains a minimum pedestrian clear width of 5 feet. Furniture not listed here may also be permitted.
  - a. Benches.
  - b. Chairs.
  - c. Tables.
  - d. Water fountains.
  - e. Trash cans.
  - f. Bike racks.
  - g. Bollards, which may include lights.
  - h. Play equipment.
  - i. Exercise equipment.
  - j. Artwork.
10. **Signage.** Designs for street signs, freestanding signs, and all other signs installed by the Developer to identify the Neighborhood may vary from the Weber County Code of Ordinances as determined by the Town Architect.

## PRIVATE REALM



The images shown in this Figure were taken in Norton Commons, a Traditional Neighborhood Development in Louisville, Kentucky. The architectural character of The West Weber Property will be similar to that of Norton Commons and will be achieved through a similar design review process. The development of Norton Commons has been shaped by a Design Code (a.k.a. Pattern Book) enforced by a Town Architect similar to this Design Code. The Design Code is based on principles of traditional architecture which regulate many aspects of the design from massing and composition to architectural details. The administration of the Design Code by a Town Architect has allowed for detailed consideration for how each design contributes to the identity of the place, resulting in a neighborhood that generates value from its walkability, mix of uses, and traditional neighborhood character.

PRIVATE REALM



Private Lot & Building Requirements by Transect Table

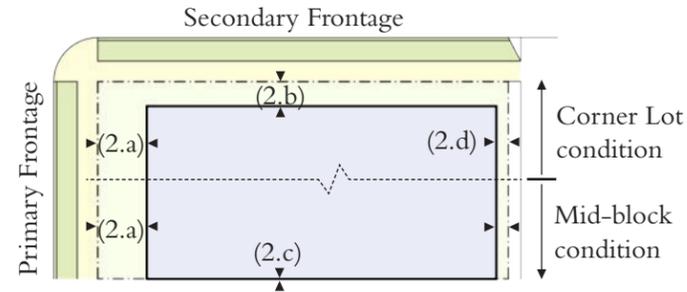
|                             | T2<br>Rural                     | T3<br>Neighborhood<br>Edge | T4<br>Neighborhood<br>General | T5<br>Neighborhood<br>Center | SD<br>Special<br>District |
|-----------------------------|---------------------------------|----------------------------|-------------------------------|------------------------------|---------------------------|
| <b>1. Lot Dimensions</b>    |                                 |                            |                               |                              |                           |
| a. Lot Width                | Determined by<br>Town Architect | 50 ft. min.                | 16 ft. min.                   | 10 ft. min.                  | See page<br>29            |
| b. Lot Coverage             |                                 | 80% max.                   | 90% max.                      | 100% max.                    |                           |
| <b>2. Building Setbacks</b> |                                 |                            |                               |                              |                           |
| a. Primary Front Setback    | Determined by<br>Town Architect | 10-40 ft.                  | 5-30 ft.                      | 0-20 ft.                     | See page<br>29            |
| b. Secondary Front Setback  |                                 | 10-40 ft.                  | 5-30 ft.                      | 0-20 ft.                     |                           |
| c. Side Setback             |                                 | 5 ft. min.                 | 0 ft. min.                    | 0 ft. min.                   |                           |
| d. Rear Setback             |                                 | 0 ft. min.                 | 0 ft. min.                    | 0 ft. min.                   |                           |
| <b>3. Building Height</b>   |                                 |                            |                               |                              |                           |
|                             |                                 | 2½ stories max.            | 3 stories max.                | 4 stories max.               |                           |
| <b>4. Frontage Types</b>    |                                 |                            |                               |                              |                           |
| a. Common Lawn              | Determined by<br>Town Architect | permitted                  | permitted                     | not permitted                | See page<br>29            |
| b. Porch                    |                                 | permitted                  | permitted                     | permitted                    |                           |
| c. Terrace & Light Court    |                                 | not permitted              | permitted                     | permitted                    |                           |
| d. Forecourt                |                                 | not permitted              | permitted                     | permitted                    |                           |
| e. Stoop                    |                                 | permitted                  | permitted                     | permitted                    |                           |
| f. Storefront               |                                 | not permitted              | permitted                     | permitted                    |                           |
| g. Gallery                  |                                 | not permitted              | permitted                     | permitted                    |                           |
| h. Arcade / Colonnade       |                                 | not permitted              | not permitted                 | permitted                    |                           |
| <b>5. Principal Uses</b>    |                                 |                            |                               |                              |                           |
| a. Residential              | not permitted                   | restricted                 | limited                       | open                         | See page<br>29            |
| b. Lodging                  | not permitted                   | restricted                 | limited                       | open                         |                           |
| c. Office                   | not permitted                   | restricted                 | limited                       | open                         |                           |
| d. Retail                   | not permitted                   | restricted                 | limited                       | open                         |                           |
| e. Agricultural             | restricted                      | restricted                 | not permitted                 | not permitted                | restricted                |
| f. Civic                    | open                            | open                       | open                          | open                         | open                      |

# PRIVATE REALM

## Private Lots & Buildings

1. **Applicability.** The requirements of this section apply to all Private Lots and buildings within the Master Plan.
2. **Lot Dimensions.** Lot Width and Lot Coverage requirements for Private Lots in each Transect Zone are listed in the Private Lot & Building Requirements by Transect Table (page 27). There shall be no minimum or maximum Lot Depth or Lot Size. Corner Lots may be up to 10 feet wider than otherwise permitted in that Transect Zone.
3. **Private Frontages.**
  - a. **Lot Frontage.** The minimum Lot Frontage shall be equivalent to the minimum Lot Width.
  - b. **Primary & Secondary Frontages.** Primary and Secondary Frontages are mapped on the Regulating Plan (page 8). Each Lot shall have at least one Primary Frontage, and the location of transitions between Primary and Secondary Frontages may be modified.
  - c. **Main Entrance.** The Main Entrance of all Principal Buildings shall be oriented to the Primary Frontage, and pedestrian access shall be provided from the Main Entrance to the adjoining Sidewalk or Pedestrian Path.
  - d. **Lots Without Frontage.** Lots without Frontage on a Front Thoroughfare or Civic Space may be permitted by determination of the Town Architect.
  - e. **Private Frontage Types.** All Principal Buildings shall include a minimum of one Private Frontage Type.
4. **Principal & Accessory Buildings.** On each Lot, one Principal Building shall be positioned within the range required by the Primary Front Setback. Lots may also include any number of Accessory Buildings. This requirement does not apply to Lots without Frontage as permitted within this Design Code.
5. **Building Setbacks.** Building Setback requirements for buildings in each Transect Zone are listed in the Private Lot & Building Requirements by Transect Table (page 27). Buildings shall be distanced from Lot Lines according to the Setbacks as illustrated in the Setback Diagram.

### Setback Diagram



### Notes

1. The label for each Setback dimension corresponds to a row of the Private Lot & Building Requirements by Transect Table (page 27).
6. **Building Height.** Building Height requirements for buildings in each Transect Zone are listed in the Private Lot & Building Requirements by Transect Table (page 27).
  - a. **Building Height Exceptions.** Towers, including but not limited to: spires, church steeples, fire towers, and scenery lofts may be exempted from these Building Height requirements by determination of the Town Architect.
  - b. **Attics and Basements.** Attics and Basements are not counted toward a building's required number of Stories.
7. **Building Size & Width.** There shall be no minimum building size or width in any Transect Zone or for any Use.
8. **Encroachments.** Permitted encroachments into Setbacks, Thoroughfares, and Civic Spaces are listed in the Permitted Encroachments Table. All encroachments into utility easements are subject to applicable easement restrictions.
9. **Parking Areas.** Parking Areas on Private Lots shall be located within the Buildable Area. Parking Areas on Private Lots shall comply with the Parking Areas requirements of this Design Code (page 29).

### Encroachments Table

|                                       | Front Setbacks | Side & Rear Setbacks | Thoroughfares & Civic Spaces |
|---------------------------------------|----------------|----------------------|------------------------------|
| Private Frontage Types <sup>1,3</sup> | •              | •                    | •                            |
| Building <sup>2,3</sup>               | •              |                      | •                            |
| Stairs & Ramps                        | •              | •                    | •                            |
| Balconies                             | •              | •                    | •                            |
| Bay Windows                           | •              | •                    | •                            |
| Towers                                | •              | •                    | •                            |
| Chimneys                              | •              | •                    | •                            |
| Eaves & Architectural Details         | •              | •                    | •                            |
| Canopies & Marquees                   | •              | •                    | •                            |
| Awnings <sup>1,3</sup>                | •              | •                    | •                            |
| Lights                                | •              | •                    | •                            |
| Fences & Walls                        | •              | •                    |                              |
| Signs <sup>1,3</sup>                  | •              | •                    | •                            |
| Planters                              | •              | •                    | •                            |
| Furniture <sup>1,3</sup>              | •              | •                    | •                            |
| Merchandise <sup>1,3</sup>            | •              | •                    | •                            |
| Patios & Decks                        |                | •                    |                              |
| Mechanical Equipment                  |                | •                    |                              |
| Utility Meters                        |                | •                    |                              |
| Window Wells                          |                | •                    |                              |

### Notes

1. These elements may encroach into Thoroughfares and Civic Spaces only if associated with a Storefront, Gallery, Arcade, or Colonnade.
2. These elements may encroach into Thoroughfares and Civic Spaces only if associated with an Arcade or Colonnade.
3. These elements may encroach within 2 feet of Vehicular Travel or Parking Lanes. A 7-foot minimum clear height shall be maintained with the exception of columns, piers, and posts. A 5-foot-wide minimum pedestrian clear zone shall also be maintained.

## PRIVATE REALM

### Special Districts

1. **Applicability.** The requirements of this section apply to all Private Lots within Special Districts as shown on the Regulating Plan (page 8) or revisions therein. Requirements in this section shall supersede any conflicting requirements in other sections.
2. **Private Lots & Buildings.** Requirements for all the items described in the Private Lots & Buildings and Uses sections of this Design Code shall be determined by the Town Architect.
3. **Uses.** All Uses permitted in T5 are also permitted in Special Districts.
4. **Frontages.** The location of Frontages shall be determined by the Town Architect and may not correspond to Lot Lines.
5. **Access.** A means of pedestrian and bicycle access shall be provided to a building's Main Entrance from within the neighborhood and from the Sidewalk at the Primary Frontage. Access may be provided through some combination of Sidewalks, crosswalks, and Paths, all with a minimum clear width of 5 feet.
6. **Parking Placement.** Parking need not be located within the Buildable Area and need not be screened from Frontages.
7. **Yards & Landscaping:** There shall be no required landscaping in Front Yards in Special Districts.

### Parking Areas

1. **Applicability.** The requirements of this section apply to all Parking Areas. Parking Lanes in Thoroughfares are not considered Parking Areas.
2. **Location & Access.** Parking Areas on Private Lots shall be located as consistent with this Design Code. Parking Areas shall be accessed by Alleys unless it is located on a Lot that does not adjoin an Alley.
3. **Size.** Head-in parking spaces shall have a minimum depth of 18 feet and a minimum width of 9 feet, with the exception of compact spaces. Drive aisles serving head-in spaces shall have a minimum width of 22 feet. The required dimensions for diagonal spaces and associated drive aisles shall be determined by the Town Architect.
4. **Tandem Parking Spaces.** Parking Areas associated with Residential Uses may include tandem parking spaces.
5. **Truck Loading.** Given the small scale of the Commercial Uses anticipated for the Neighborhood, truck loading shall typically be accommodated via Alleys and Commercial Streets. If required, the design of off-street truck loading spaces shall be determined by the Town Architect.
6. **Bicycle Parking.** The quantity and design of parking for bicycles shall be determined by the Town Architect.
7. **Parking Lots.** The following requirements apply only to Parking Lots:
  - a. **Connectivity.** Parking Lots on adjacent Private Lots shall connect to one another.
  - b. **Landscaping.** One tree island (of 100 square feet minimum with one 2-inch minimum caliper tree and ground cover) is required for every 10 spaces that are side-by-side. Alternatively, an equivalent number of trees may be provided at the perimeter of the Parking Lot. Parking Lots of fewer than 50 spaces and/or those of no more than one row of double-loaded head-in parking are exempt from landscaping requirements. Landscaping in Parking Lots shall comply with the requirements for Yards & Landscaping (page 29).
  - c. **Paving Materials.** Asphalt, stabilized decomposed granite, limestone, concrete, brick, Grasscrete (and similar products), integral colored concrete, stamped concrete, granite, granite pavers, cobblestone, decomposed granite, tar and chip, asphalt pavers, brick pavers, concrete pavers, and permeable pavers may be used. Other materials may also be permitted.
8. **Bumpers.** Bumpers may or may not be included in the design of Parking Areas.
9. **Light Spillover.** Lighting on one Parking Area may spill over onto a Parking Area on an adjacent lot.
10. **Screening.** Parking Areas shall be screened from Frontages with landscaping, fences, or walls, as determined by the Town Architect.

### Yards & Landscaping

1. **Applicability.** The requirements of this section apply to all Yards and to all landscaping on Private Lots. Requirements for landscaping in Thoroughfares and Civic Spaces are listed in the Public Realm section of this Design Code.
2. **Species.** The species used in landscaping for all Yards shall conform to the Approved Plant List and the Prohibited Plant List in the Weber County Code of Ordinances. Additional species may be permitted by the Town Architect only if they are not listed on the Prohibited Plant List.
3. **Swimming Pools.** Swimming pools may be located in Side or Rear Yards as permitted by the Design Code as described in this Design Code.
4. **Design Code.** Additional regulations for Yards and landscaping will be included in the Design Code as described in this Design Code.

### Lighting

1. **Applicability.** The requirements of this section apply to all lighting on Private Lots. Requirements for lighting in Thoroughfares and Civic Spaces are listed in the Public Realm Details section of this Design Code.
2. **Shielding.** All light fixtures shall be fully shielded and direct light downward, with the exception of lighting for signs or art as consistent with this Design Code and the Design Code.
3. **Spillover.** Light need not be directed away from adjacent Thoroughfares, Civic Spaces, and Front Yards.
4. **Height.** Light pole height shall not exceed 25 feet. The height of wall-mounted light fixtures shall not exceed 30 feet, with the exception of lighting for signs as consistent with this Design Code and the Design Code.
5. **Design Code.** Additional regulations for Yards and landscaping will be included in the Design Code as described in this Design Code.