



Staff Report to the Ogden Valley Planning Commission

Weber County Planning Division

Synopsis

Application Information

Application Request:	A public hearing to consider and make recommendation on a new ordinance for the Eden Mixed-Use Zoning regulations.
Agenda Date:	October 26, 2021
Applicant:	Weber County
File Number:	ZTA2019-03

Staff Information

Report Presenter:	Steve Burton sburton@co.weber.ut.us 801-399-8766
Report Reviewer:	CE

Applicable Ordinances

- Title 101, Chapter 1, General Provisions, Section 7, Definitions
- Utah State Code, Title 17, Chapter 27a, Part 5 Land Use Regulation

Background and Summary

This is a county initiated proposal to adopt an ordinance establishing the Eden Village Mixed-Use Zone. The 2016 Ogden Valley General Plan provides goals and policies that state the County should adopt specific area plans for the commercial villages including the Old Town Eden and New Town Eden area. This new zoning ordinance will ensure that development within the Eden Villages focuses on building architecture and street design to result in pedestrian friendly retail.

In November of 2018, planning staff met with stakeholders in the Eden commercial areas to discuss the idea of form based zoning in Eden. The feedback from the stakeholders was that the Old and New Town Eden areas should have stricter development standards that require enhanced building “main street” architecture, similar to the Eden blacksmith shop. In addition to enhanced architecture, the stakeholders suggested a street design that focuses on pedestrians, cyclists, and retail customers.

The proposed Eden Village Mixed-Use zone requires street front buildings to have a façade that has specific architecture, materials, and colors. The Old Town Eden area will have street front façades with architecture, materials, and colors that resemble western main street buildings from the late 1890s to the early 1910s. Currently, the blacksmith shop and the general store meet these requirements with either brick or wood fronts that hide gable roofs and provide other important architectural detail. The New Town Eden area will have agrarian style buildings, resembling historic barns that will also have specific architectural detail.

The public streets in these areas will be wide enough to support spacious pedestrian sidewalks (approximately 14 feet), bike lanes (approximately five feet), and on street parking (either angled or parallel). Traffic calming measures including street trees and intersection bulb outs will be required. The width of drive lanes will be the county public works standard of 12 feet.

Staff Recommendation

Staff recommends that the Planning Commission forward a positive recommendation of ZTA 2019-03 to the County Commission. This recommendation is based on the following findings:

1. The proposed subdivision amendment conforms to the Ogden Valley General Plan.

Exhibits

A. Draft Ordinance

1 [104-31-1 Purpose and intent](#)

2 [The purpose of the Eden Village Zone is to provide specific development standards that result in “Main](#)
3 [Street” commercial corridors with multi-modal active transportation and enhanced building design. The](#)
4 [Eden Village Zoning regulations are intended to carry out the objectives of the 2016 Ogden Valley](#)
5 [General Plan through the implementation of form based, small area zoning.](#)

6

7 [104-31-2 Applicability](#)

8 [The principles, standards, and guidelines in this chapter apply to proposals for new development,](#)
9 [exterior alterations, and site improvements to existing buildings. Except when more specific regulations](#)
10 [are provided in this chapter, the CV-2 zoning regulations and the Design Review regulations apply to](#)
11 [projects in the Eden Village Zone.](#)

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13 [104-31-3 Permitted uses](#)

14 [The uses outlined in the CV-2 zone, except for automobile intensive uses, are permitted in the Eden](#)
15 [Village Zone. In addition to the uses listed in the CV-2 zones, dwelling units are permitted above the first](#)
16 [floor of any retail space that was developed within the regulations of the Eden Village Zone.](#)

17 [104-31-4 Site Development Standards:](#)

Site Development Standards	
Minimum Lot Area	<i>None</i>
Minimum Lot Width	<i>None</i>
Maximum front yard setback for first-story street-facing commercial space.	<i>5 feet, except more may be provided for patio or dining area.</i>
Minimum front yard setback for all other buildings.	<i>None, provided however, any building proposed to be setback more than 5 feet shall be setback a minimum of 60 feet.</i>
Minimum side yard setback	<i>None, unless the side lot line is within 10 feet of an agricultural or residential zone, in which case the setback is 10 feet.</i>
Minimum rear yard setback	<i>None, unless the rear lot line is within 10 feet of an agricultural or residential zone, in which case the setback is 10 feet.</i>
Maximum building height	<i>45 feet;</i>

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<u>Minimum building depth for First-story street-facing commercial space</u>	<u>30 feet;</u>
<u>Minimum building width for first-story street-facing commercial space</u>	<u>75 percent of a lot's street frontage.</u>
<u>Minimum building fenestration for first-story street-facing commercial space</u>	<u>65 percent of the area of the façade.</u>

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19 104-31-5 Additional Development Standards

- 20 (a) Drive-up (drive-thru) window. Any business with a drive-up (drive-thru) window shall locate the
 21 window in an area of the building that is not visible from the public street. The stacking lanes
 22 and drive through parking spaces shall also be located in an area that is not visible from the
 23 public street.
 24 (b) Dwelling Units. Dwelling units are permitted above first floor retail space through transferrable
 25 density rights. Transfers of units from areas designated as "transfer areas" are encouraged to be
 26 received within the Eden Village Area. Dwelling unit square footage, shall not exceed the square
 27 footage of the first floor retail space.

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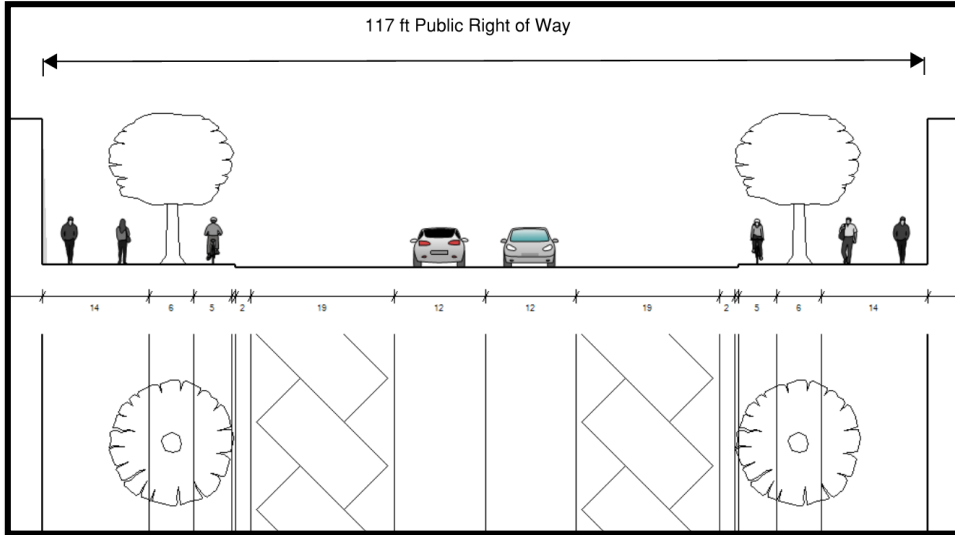
29 104-31-6 Street Design

- 30 (a) Public right-of-way. As development occurs on each parcel, the owner shall dedicate area for
 31 public right of way to form a block pattern within the village area as depicted by the street cross
 32 section in figure 1.1.

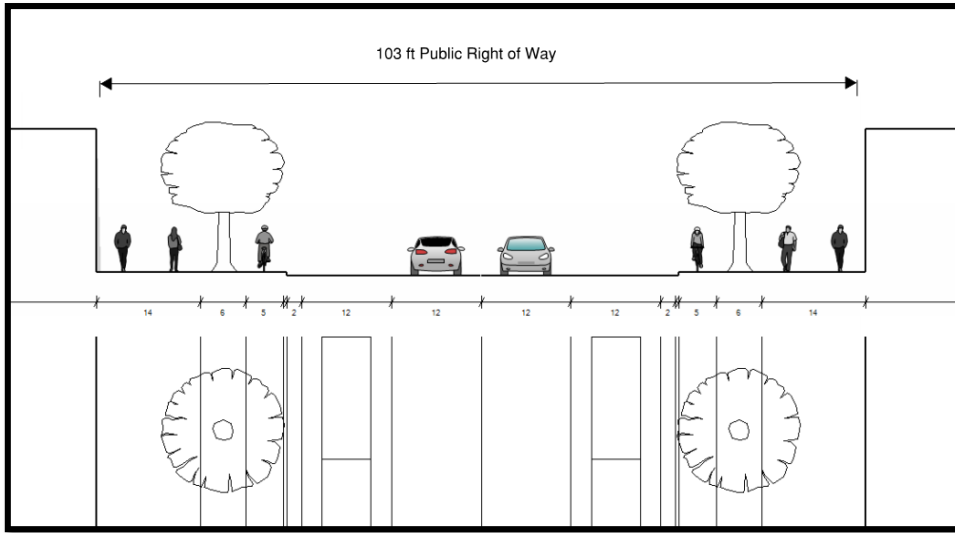
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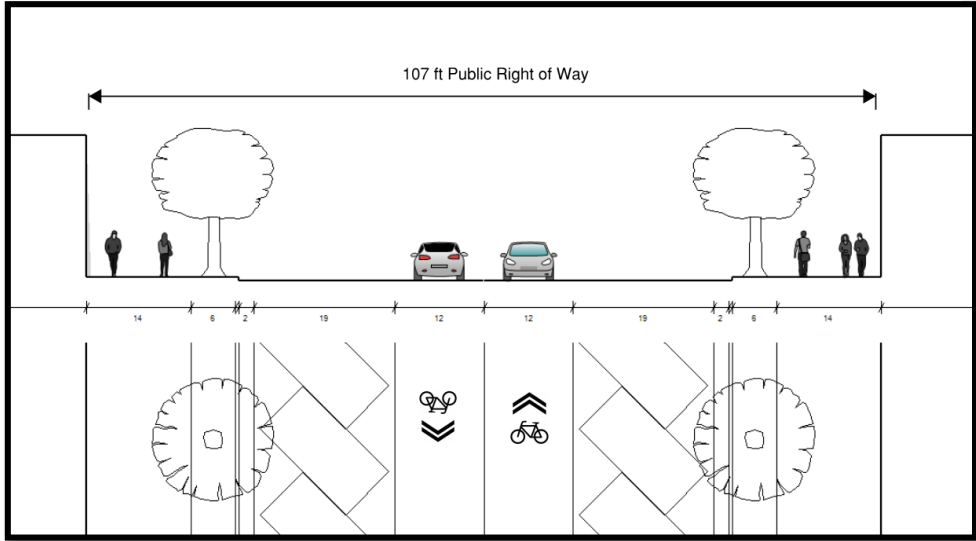
33 Figure 1.1

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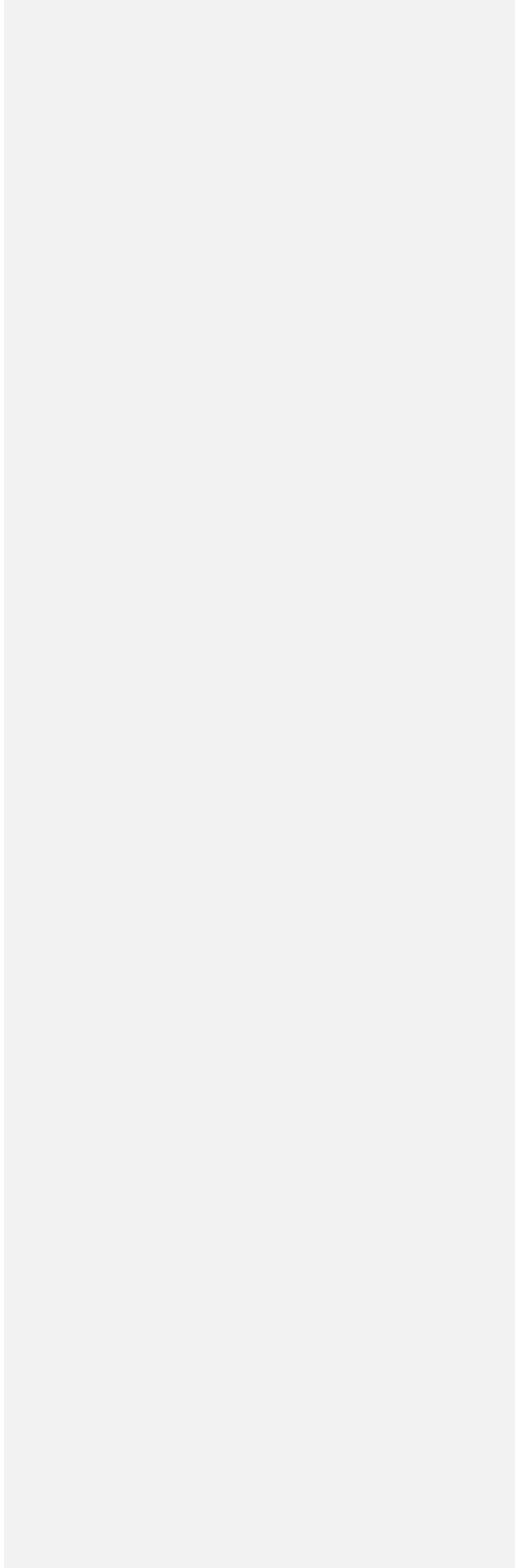
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[\(b\) Mid-block alleyways. As development occurs on each parcel, sufficient area shall be preserved for the formation of internal block alleyways that provide internal access to parking areas as depicted by figure 1.2 Below](#)

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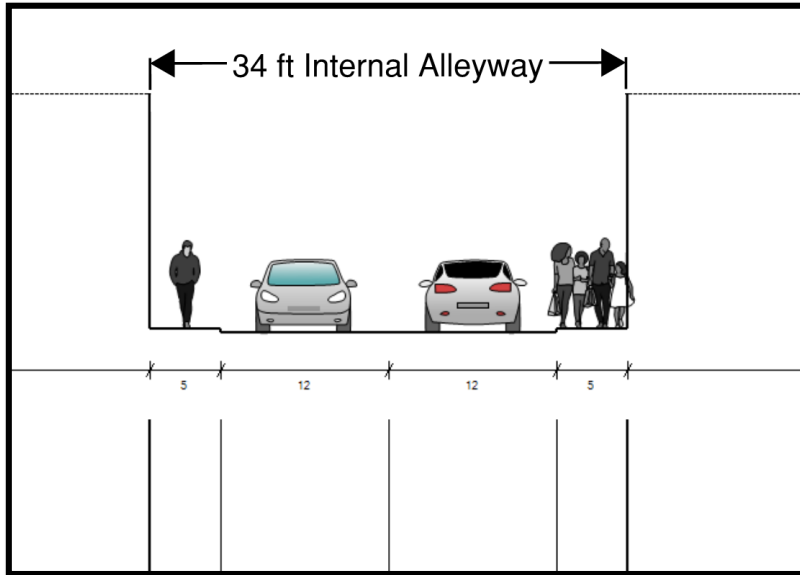


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(1) The following cross section shall be implemented as internal alleyways develop:



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(c) Parking.

(1) Each application for development shall include a parking plan, which demonstrates that sufficient parking will be provided by the angled parking in front of each building and off-street parking lots which are accessed from internal alleyways. All parking facilities must be paved and asphalted. On-street angled parking spaces not adjacent to the site shall not be used to determine that sufficient parking has been provided.

(2) Each site must provide on-site parking area in addition to the on-street angled parking that complies with the following:

- (a) On-site parking areas shall only be accessed through internal alley ways;
- (b) On-site parking areas shall be located so that when the block is developed, the parking area is screened by buildings on all sides, and can only be seen from internal alley ways.
- (c) The applicant shall provide evidence that the proposed number of on-site parking spaces will be sufficient to accommodate the proposed use without affecting existing parking infrastructure.

(d) Street Trees. Street trees shall be installed as part of the required street improvements within the Eden Village Zone. Tree species shall be approved by the Planning Director and the County Engineer, as part of building permit review. A street tree plan shall be submitted as part of a building permit application and shall be accompanied by a letter from a certified arborist or landscape architect, certifying that the proposed tree type is suitable considering soil types in

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76 the area. The street tree plan shall also include a planting method to be approved by the
77 Planning Director and the County Engineer.

78 (1) The following regulations apply to the planting of street trees:

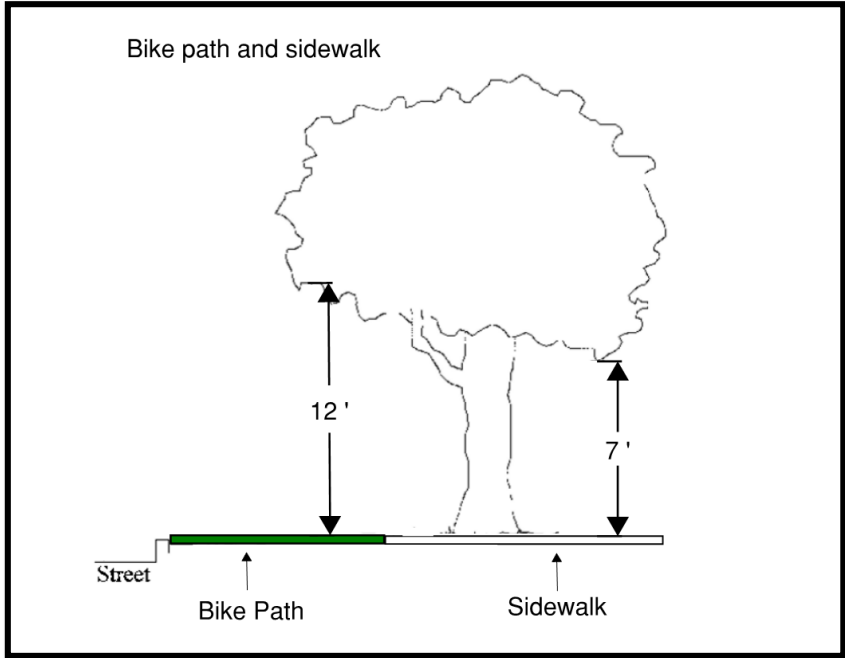
- 79 (a) All trees shall be planted within the center of the required sidewalk along the public
80 right-of-way.
- 81 (b) At street intersections, no street tree shall be planted within forty feet (40') of the
82 vertex (the corner point if the 2 curbs intersect in straight lines rather than having a
83 radius) of any corner within an intersection.
- 84 (c) No street tree shall be planted within fourteen feet (14') of any building, structure
85 or fence, unless otherwise specified by the Planning Director and County Engineer.
- 86 (d) No street tree shall be planted within fifteen feet (15') of a streetlight, utility pole,
87 driveway or alley.
- 88 (e) No street tree shall be planted within eight feet (8') of any water meter.
- 89 (f) In general, minimum spacing between trees to achieve optimum individual growth
90 rates when canopies mature shall be fifty feet (50'). The Planning Director and
91 County Engineer may make specific spacing requirements for street tree plantings
92 based upon individual site condition/characteristics and species or varieties
93 characteristics and requirements.

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95 (2) Tree Maintenance. Street trees shall be maintained by the owner or proprietor of
96 property that has frontage along the public street in which the trees are planted. A tree
97 maintenance plan shall be submitted as part of a building permit application for new
98 development within the Eden Village Zone. Trees shall be pruned in a manner that
99 leaves a 7 foot clearance above sidewalk and a 12 foot clearance above the bike path
100 and parking areas.

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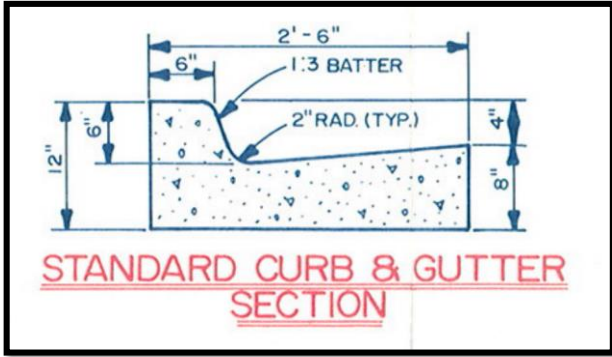


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(e) [Sidewalk. A fourteen \(14\) foot sidewalk shall be installed as part of the required street improvements. Sidewalk material shall be concrete and shall blend in to existing sidewalk that was installed as required by this chapter.](#)

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110 (f) Curb and Gutter. Curb and Gutter shall be installed in the public right-of-way, and internal
111 alleyways, in accordance with the County's standard Curb and Gutter Cross Section.



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117 (g) Bike Path. An asphalt bike path 5 feet in width shall be installed as part of the required street
118 improvements.

119 (h) Angled parking. 45 degree angled parking shall be provided within the public right-of-way as
120 part of the required street improvements. Angled parking shall follow the width and angle
121 standards shown in figure 1.1.

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124 (i) Curb Extension Bulb outs. In order to provide traffic calming measures and pedestrian safety,
125 developments at intersections shall install curb extension bulb-out sidewalks. Curb extensions
126 shall also be provided at the mid-block point along each block. Sidewalk and bike path widths
127 shall not be obstructed or made narrower at any point along curb extensions. Owners will be
128 required to submit engineered drawings of curb extension bulb outs as part of site plan review.
129 The following images are examples of bulb-outs.
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139 [\(j\) Projections. Building projections such as awnings and canopies that are intended to meet the](#)
140 [architectural requirements of this chapter are permitted within the public right-of-way.](#)
141 [Projections shall leave a vertical clearance of 10 feet above the side-walk and shall not project](#)
142 [more than 7 feet into the right of way.](#)

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143 (k) Temporary structures. Temporary structures that are intended to provide amenities to
144 pedestrians and cyclists, including but not limited to street furniture for outside dining, benches,
145 and bike racks are permitted in the public right-of-way. Temporary structures shall not project
146 more than 7 feet into the public right-of-way and shall be placed at a location that does not
147 obstruct pedestrian access.

148 104-31-7 Building Architecture (OLD TOWN)

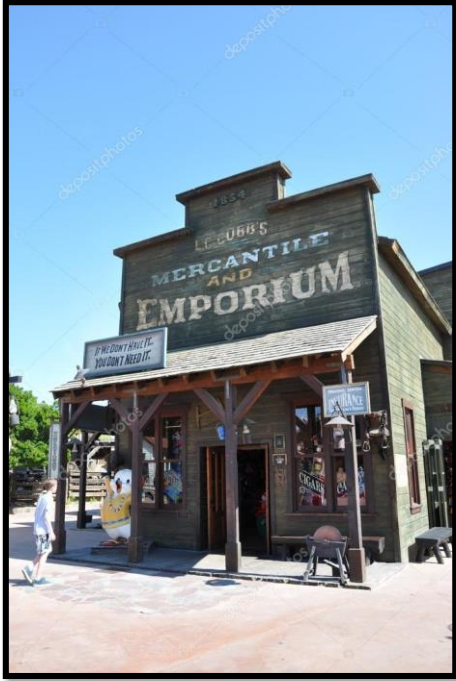
149 (a) Within the Old Town Eden Village Area, street front buildings shall have architectural styles and
150 materials that resemble historic commercial main street buildings from the era of the late 1880s
151 to the early 1910s. Each new building shall provide diversity in building design to any existing,
152 adjacent buildings. The building fronts shall consist of brick or wood, and shall include a vertical
153 facade to hide gable roofs and to break-up long horizontal lines. The following images are
154 examples of appropriate architecture and colors from the era:

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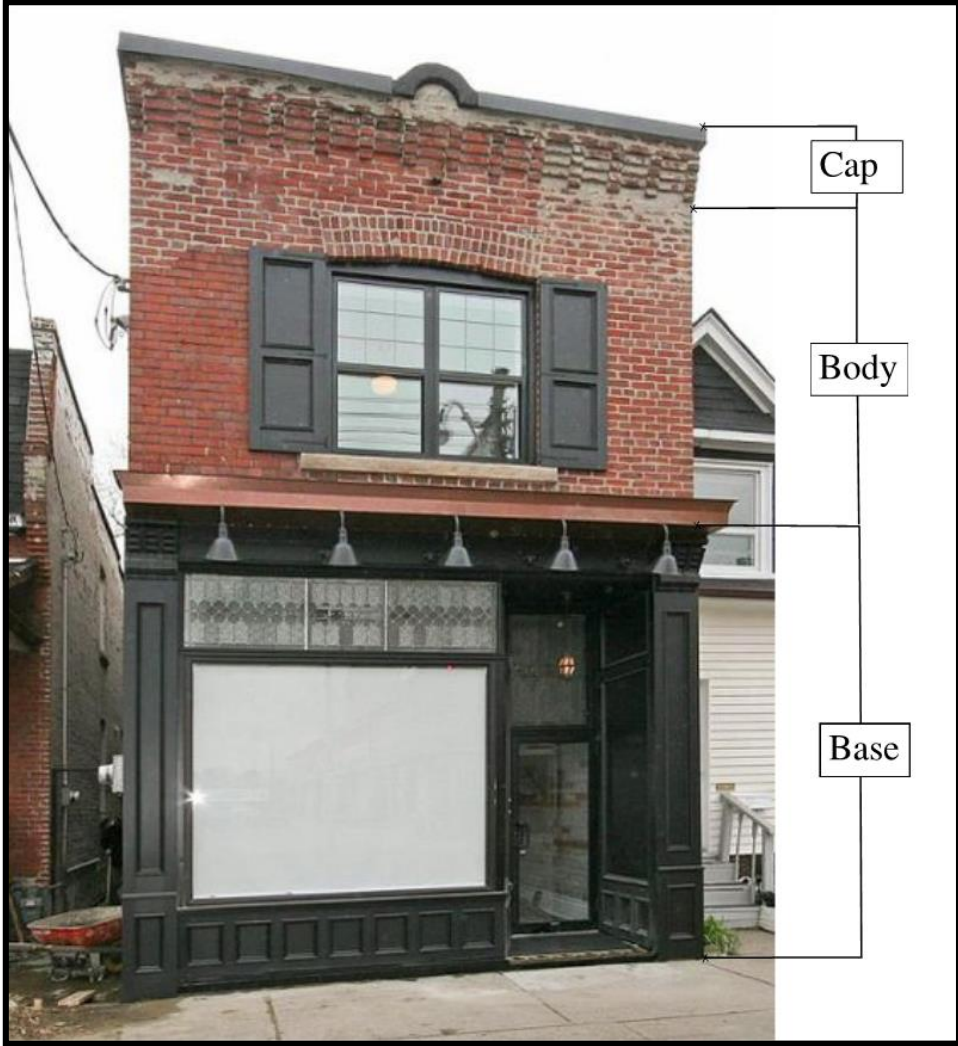
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(b) Base, Body, and Cap. Each street front building shall incorporate a building base, body, and cap into the design of the building façade. The following image is an example of a façade that incorporates the base, body, and cap elements:

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168 [\(c\) Colors. The color of building materials shall also represent the era of the late 1890s to the early](#)
 169 [1910s. The natural colors of wood and brick and the natural metal patina are examples of](#)
 170 [acceptable colors. Other colors may be used as accent colors.](#)

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172 [104-31-8 Building Architecture \(New Town\)](#)

173 [\(a\) Within the New Town Eden Village area, buildings shall implement agrarian style architecture.](#)
 174 [Buildings in New Town Eden shall incorporate barn elements such as pitched roofs and wood or](#)
 175 [metal siding. In New Town Eden, street facing building facades that do not use natural wood](#)
 176 [siding shall incorporate a brick or stone wainscot as a building base. The following are examples](#)
 177 [of appropriate architecture and materials:](#)

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184 (b) Colors. The color of buildings within New Town Eden shall be earth tone natural colors. Red may
185 also be used as a primary building color. White shall be used as an accent color around windows
186 and other openings.

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188 104-31-9 Guarantee of improvements:

189 Unless installed before obtaining a building permit, the owner shall guarantee the completion of the
190 required street improvements in the form of a cash escrow to the County. The owner will be required to
191 submit to the County a cost estimate from a professional engineer licensed in the state of Utah.

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